

SCHEDULE A: Applications with Recommendation

09/0978

Item No: 02

Date of Committee: 29/01/2010

Appn Ref No:
09/0978

Applicant:
Possfund Custodian
Trustees Limited

Parish:
Carlisle

Date of Receipt:
06/11/2009

Agent:
Savills

Ward:
Currock

Location:
St Nicholas Retail Park, St Nicholas Gate, St
Nicholas, Carlisle

Grid Reference:
340834 555100

Proposal: Alterations To Existing Retail Units, Part Demolition And Erection Of New Retail Warehousing (Reserved Matters Application Pursuant To Outline Permission 03/1362)

Amendment:

REPORT

Case Officer: Richard Maunsell

Reason for Determination by Committee:

This application is brought for determination by Members of the Development Control Committee because objections have been received from the Conservation Area Advisory Committee.

1. Constraints and Planning Policies

Conservation Area

The proposal relates to land or premises situated within the Botchergate Conservation Area.

Gas Pipeline Safeguarding Area

The proposal relates to land or premises situated within or adjacent to the Gas Pipeline Safeguarding Area.

RSS Pol DP 1 - Spatial Principles

RSS Pol DP 3 - Promote Sustainable Economic Development

RSS Pol DP 5 - Manage Travel Demand. Reduce Need to Travel

RSS Pol DP 7 - Promote Environmental Quality

RSS Pol W 5 - Retail Development

RSS Pol CNL 2 - Sub-area Development Priorities for Cumbria

Local Plan Pol DP1 - Sustainable Development Location

Local Plan Pol DP2 - Regeneration

Local Plan Pol CP3 - Trees and Hedges on Development Sites

Local Plan Pol CP5 - Design

Local Plan Pol CP6 - Residential Amenity

Local Plan Pol CP9 - Devel., Energy Conservation and Effic.

Local Plan CP15 - Access, Mobility and Inclusion

Local Plan Pol CP16 -Public Trans.Pedestrians & Cyclists

Local Plan Pol CP17 - Planning Out Crime

Local Plan Pol EC5 - Large Stores and Retail Warehouses

Local Plan Pol EC8 - Shopfronts

Local Plan Pol LE12 - Proposals Affecting Listed Buildings

Local Plan Pol LE19 - Conservation Areas

Local Plan Pol T1- Parking Guidelines for Development

2. Summary of Consultation Responses

Cumbria County Council - (Highway Authority): no objection subject to the imposition of conditions;

Community Services - Drainage Engineer: comments awaited;

Cumbria County Council - (Archaeological Services): the comments that refer to the earlier application are still applicable.

It is therefore recommended that an archaeological investigation of the site be undertaken in advance of development and advise that these works be secured by attaching a negative condition to any consent;

Cumbria County Council (Strategic Planning Authority) Wind Energy Consultations: comments awaited;

Development Services Planning & Housing Services - Access Officer: comments awaited;

Cumbria Constabulary - Crime Prevention: Cumbria Constabulary is encouraged to note the frequent references to security and safety in the submitted Design and Access Statement and the additional Planning Statement. It is evident that the applicants have acknowledged the persistent anti-social and criminal activity that occurs on this site, which may have an adverse impact on the sustainability of their estate.

Cumbria Constabulary has carried out a brief search of police records between 1 January 2006 and 30th November 2009. St Nicholas Retail Park generated 880 calls for police service over this period – a considerable number for a relatively small area. Police were requested to deal with all manner of incidents, ranging from nuisance gathering, complaints regarding manner of driving, road traffic collisions and intruder alarm activations. There were 70 recorded offences, mainly relating to retail theft (i.e. shoplifting), malicious damage and public order.

Perimeter Security

Cumbria Constabulary notes the intention to improve the security of the rear service yard and to reinforcing the boundary with the adjacent railway. However, the proposed fencing type or specification is not indicated.

It is therefore recommend the incorporation of a welded mesh or extruded metal fence, of at least 2.4m and installed to BS 1722-14:2006. Access gates should be of similar material and secured by padlocks conforming to BS EN 12320 or an alternative security standard locking mechanism.

Consideration should be given to closing the designated vehicle access point at the end of trading each day, to prevent misuse by motor vehicles and unauthorised HGV parking.

Access Points

Cumbria Constabulary supports the proposal to dispense with the designated vehicle access points via St Nicholas Road and adjacent to Halfords unit. This measure shall prevent motor vehicles short-cutting across the site to avoid congestion. The profusion of access points is continually exploited by persons involved in the misuse of motor vehicles on the site.

Car Parking

The intention to re-align and rationalise the car parking provision is noted. Twelve offences relate to offences against motor vehicles. The developer must reduce the opportunities for vehicle crime occurring, otherwise potential customers will be discouraged from visiting the site. Cumbria Constabulary would be pleased to assist the developer to participate in the Safer Parking Scheme, the national initiative to address vehicle crime. Further details can be supplied on request.

Cycle Parking

In order to support sustainable transport links, the intention to provide cycle parking on the site is noted. Unfortunately, pedal cycle theft continues to be a persistent problem across the City. Cycle parking must therefore be perceived to be safe and secure to encourage legitimate use.

The use of 'Sheffield' type stands is considered to represent best practice, in order that owners can chain or 'D-lock' the cycle frame and wheel to a substantial feature. Cycle parking must be in prominent and 'active' locations to maximise surveillance opportunities. Protection from adverse weather is desired, but the manner in which this is provided must not compromise surveillance or user reassurance.

CCTV

The DAS makes reference to a S106 Agreement for the provision and maintenance of a CCTV camera system, linking to the Local Authority scheme. However, the Planning Statement intimates that the S106 Agreement refers only to the provision and maintenance of just one camera. This matter requires clarification.

The present CCTV system (observing the exterior spaces) is reportedly constantly monitored, yet I am aware that several previous enquires made by police to acquire video evidence for various matters have been thwarted by the system not being fully operational. This situation is unacceptable and compromises the ability of the police to investigate crime. It also reflects adversely on the site owners to provide a safe and secure environment for their customers.

In this regard, it is requested that a condition be imposed on this application regarding the proposals for CCTV coverage of the external spaces of this site.

Cash Handling

The DAS gives no indication of proposals for cash handling facilities or the safe reception of Cash-in-Transit vehicles. Further clarification is sought on these issues.

Landscaping

The site shall be extensively landscaped in order to provide an aesthetically pleasing environment. The landscaping scheme must be designed in conjunction with the proposed lighting scheme and the CCTV proposals to avoid potential conflict.

The present landscaping scheme impedes CCTV views (particularly noticeable when trees are in leaf).

Security Lighting

The information supplied does not indicate the proposals for this measure.

Physical Security

With regard to the construction and refurbishment of the retail buildings, Cumbria Constabulary recommends the incorporation of exterior door (including roller shutters) and window products compliant with LPS 1175. This measure is intended to increase the resistance of each building shell to forced entry.

It is also recommended that each unit shall be fitted with an Intruder Alarm System, conforming to EN 50131 (Level 2) prompting a police response;

Urban Designer (Carlisle Renaissance) formerly in Dev Services - PIng & Hsg: previous pre-application comments were made in February 2007 which broadly welcomed the proposal. As previously, these proposals offer the opportunity to improve the appearance of a portion of London Road, which it is considered the proposed demolition, refurbishment and reconstruction could achieve.

Landscaping - Additional tree planting is suggested which, along with the reconfigured units, will bring greater definition to the street. It is important however that this frontage planting is of the right species and structure and it is strongly recommended that this landscaping element is revisited to provide the maximum feasible quantity of structural avenue tree planting on this frontage. The Council's adopted Urban Design & Public Realm Framework SPD suggests that in the Botchergate/London Road area this planting should be boulevard in character with an appropriate urban scale. It is suggested that off-site landscaping works to the east of London Road should be secured to compliment those on the western retail park frontage. The need for new planting relates in particular to alongside Unit 8 and to the eastern side of London Road where there are opportunities for the pit-planting of street trees in accordance with the UD&PRF SPD. Structural planting will make a far greater contribution to setting of the retail park than shrubs etc and existing inappropriate landscaping such as the grass verge to the south eastern perimeter of the site could usefully be eliminated in favour of avenue tree planting/perennial planting an/or hard landscaping. The level of detail in both the landscaping plan and the design statement makes it unclear as to how these elements will be tackled.

Existing Service Yard – This service yard is currently softened by existing tree planting. They impact positively on the appearance of Carlisle as approached via rail. It is strongly recommended that their retention is secured, or where this is not possible because of revised layout, substitutes are provided within the service area.

Pedestrian movement around and through the site – The existing crossing at the entrance to the site off London Rd observes the pedestrian desire line. The proposed crossing dictates that the pedestrian enters a chicane of guard-railing. As evidenced at the B&Q retail park further south on London Road, this results in a

completely inadequate pedestrian environment where the layout is dictated solely by the needs of the private automobile. The crossing here should be revised to create a crossing that is convenient for the pedestrian, and which is free from pedestrian barriers. The adjacent crossing over London Road, which has been highlighted in relation to the earlier scheme as again requiring pedestrians to take a convoluted route while constrained behind guardrails, could also usefully be revised via off-site contributions arising from this proposal.

In conclusion the proposal is broadly welcomed but strongly recommend that revisions are sought to the landscaping proposals abutting London Rd and to the pedestrian access arrangements;

Network Rail: with reference to the protection of the railway, Network Rail has no objection in principle to the development, but below are some requirements which must be met.

Drainage

All surface and foul water arising from the proposed works must be collected and diverted away from Network Rail property. In the absence of detailed plans all soakaways must be located so as to discharge away from the railway infrastructure.

Fail Safe Use of Crane and Plant

All operations, including the use of cranes or other mechanical plant working adjacent to Network Rail's property, must at all times be carried out in a "fail safe" manner such that in the event of mishandling, collapse or failure, no materials or plant are capable of falling within 3.0m of the nearest rail of the adjacent railway line, or where the railway is electrified, within 3.0m of overhead electrical equipment or supports.

Excavations/Earthworks

All excavations/ earthworks carried out in the vicinity of Network Rail property/ structures must be designed and executed such that no interference with the integrity of that property/ structure can occur. If temporary works compounds are to be located adjacent to the operational railway, these should be included in a method statement for approval by Network Rail. Prior to commencement of works, full details of excavations and earthworks to be carried out near the railway undertaker's boundary fence should be submitted for the approval of the Local Planning Authority acting in consultation with the railway undertaker and the works shall only be carried out in accordance with the approved details. Where development may affect the railway, consultation with the Outside Parties Engineer should be undertaken.

Security of Mutual Boundary

Security of the railway boundary will require to be maintained at all times. If the works require temporary or permanent alterations to the mutual boundary the applicant must contact Network Rail's Territory Outside Parties Engineer.

Armco Safety Barriers

An Armco or similar barrier should be located in positions where vehicles may be in a position to drive into or roll onto the railway or damage the lineside fencing. Network Rail's existing fencing / wall must not be removed or damaged. Given the considerable number of vehicle movements likely provision should be made at each

turning area/roadway/ car parking area adjacent to the railway in particular to the southeast corner of the development.

Fencing

Because of the nature of the proposed developments we consider that there will be an increased risk of trespass onto the railway. The Developer must provide a suitable trespass proof fence adjacent to Network Rail's boundary (minimum approx. 1.8m high) and make provision for its future maintenance and renewal. Network Rail's existing fencing / wall must not be removed or damaged.

Method Statements/Fail Safe/Possessions

Method statements may require to be submitted to Network Rail's Territory Outside Parties Engineer at the below address for approval prior to works commencing on site. Where appropriate an asset protection agreement will have to be entered into. Where any works cannot be carried out in a "fail-safe" manner, it will be necessary to restrict those works to periods when the railway is closed to rail traffic i.e. "possession" which must be booked via Network Rail's Territory Outside Parties Engineer and are subject to a minimum prior notice period for booking of 20 weeks. Generally if excavations/piling/buildings are to be located within 10m of the railway boundary a method statement should be submitted for NR approval.

Lighting

Where new lighting is to be erected adjacent to the operational railway the potential for train drivers to be dazzled must be eliminated. In addition the location and colour of lights must not give rise to the potential for confusion with the signalling arrangements on the railway. Detail of any external lighting should be provided as a condition if not already indicated on the application.

Standard lighting condition: For the first three months following the installation and operation of the new lighting an assessment will be made to check the effect of the lighting on the nearby railway line. If it is found that there is a problem with driver visibility additional screening/ cowling or light adjustment will be employed as appropriate to alleviate the problem, to the satisfaction of the local planning authority in association with Network Rail and the train operating companies

Access to Railway

All roads, paths or ways providing access to any part of the railway undertaker's land shall be kept open at all times during and after the development.

Network Rail is required to recover all reasonable costs associated with facilitating these works.

It is realised that much of the above does not apply directly to the application but should be taken into consideration as appropriate. Nevertheless it gives a useful guide as to the considerations to be taken into account in relation to development adjacent to the railway. It is advised that in particular the boundary fencing, Armco barriers, method statements and lighting should be the subject of conditions, the reasons for which can include the safety, operational needs and integrity of the railway. For the other matters we would be pleased if an informative could be attached to the decision notice.

The method statement will need to be agreed with Network Rail's Territory Outside Parties Engineer (London North Western);

Northern Gas Networks: no objection however there may be apparatus in the area that may be at risk during construction works and should the planning application be approved, then United Utilities may require the promoter of these works to contact Northern Gas Networks to discuss the requirements; and

Conservation Area Advisory Committee: this scheme is a revamp of the existing structures and includes additional units which will intensify the use of the site. The Committee is disappointed by the poor design of the proposal which actually lost some of the better characteristics of the existing scheme. The new units are standard fare for any industrial estate and take no notice of the fact that the site is bounded on two of its three sides by Conservation Areas. One of these is the Settle to Carlisle Conservation Area, recently named as the second best railway line in the world, and this route into the City deserves to be taken seriously. The rear elevations and the treatment of the rear boundaries need to be done in a way that improves and enhances this part of the Conservation Area. This is an opportunity to enhance the experience of visitors coming into Carlisle on this line and it should not be wasted. The rear will presumably be used for storage, refuse and a compactor. All of this needs to be screened properly from the Railway Line and the St Nicholas Viaduct.

In addition it is considered that the opportunity to improve the London Road route into Carlisle is also being lost. The new units replacing Halfords and the Tyre Distributor consisted of flat, bland elevations that created a negative impact and were no better than the existing and some elements were actually worse. These should either be redesigned or removed from the scheme and left as car parking.

3. Summary of Representations

Representations Received

Initial:	Consulted:	Reply Type:
1 Woodruffe Terrace	16/11/09	Undelivered
2 Woodruffe Terrace	16/11/09	
87 London Road	16/11/09	
89 London Road	16/11/09	
91 London Road	16/11/09	
93 London Road	16/11/09	
95 London Road	16/11/09	
101 London Road	16/11/09	
111 London Road	16/11/09	Undelivered
113 London Road	16/11/09	
115 London Road	16/11/09	
117 London Road	16/11/09	Undelivered
119 London Road	16/11/09	
121 London Road	16/11/09	
123 London Road	16/11/09	
125 London Road	16/11/09	
3 Woodruffe Terrace	16/11/09	
4 Woodruffe Terrace	16/11/09	

5 Woodruffe Terrace	16/11/09	
6 Woodruffe Terrace	16/11/09	
7 Woodruffe Terrace	16/11/09	
8 Woodruffe Terrace	16/11/09	
9 Woodruffe Terrace	16/11/09	
10 Woodruffe Terrace	16/11/09	
11 Woodruffe Terrace	16/11/09	
12 Woodruffe Terrace	16/11/09	Undelivered
47 London Road	16/11/09	
59 London Road	16/11/09	
61 London Road	16/11/09	
63 London Road	16/11/09	
65 London Road	16/11/09	
67 London Road	16/11/09	
69 London Road	16/11/09	
71 London Road	16/11/09	
73 London Road	16/11/09	
75 London Road	16/11/09	
77 London Road	16/11/09	
79 London Road	16/11/09	
81 London Road	16/11/09	
85 London Road	16/11/09	

3.1 This application has been advertised by means of three site notices, a press notice and direct notification to the occupiers of forty of the neighbouring properties. At the time of writing this report, no representations have been received.

4. Planning History

- 4.1 In 1988, planning permission was granted for the erection of a retail warehouse park with associated restaurant, car parking and landscaping.
- 4.2 Planning consent was granted in 1989 for revised access arrangements and layout for the retail warehouse park.
- 4.3 In 1996, planning permission was granted for the change of use of existing service bays to car showroom with alterations to the existing shopfront.
- 4.4 Advertisement consent was granted in 1997 for the erection of 2no. internally illuminated tenant name fascia signs, 2no. non-illuminated services fascia signs and the addition of trough lighting to existing free-standing forecourt sign.
- 4.5 In 1998, advertisement consent was granted for the erection of 2no. free standing post signs.
- 4.6 Advertisement consent was granted in 2002 for the erection of 10no. signs to replace existing corporate signage.

- 4.7 In 2006, planning permission was granted for alterations to existing retail units, part demolition and erection of new retail warehousing for the retail sale of bulky goods.
- 4.8 Advertisement consent was granted in 2007 for the erection of 2no. illuminated "6 Sheet" poster display panels.

5. Details of Proposal/Officer Appraisal

Introduction

- 5.1 This application seeks Reserved Matters approval for the alterations to the existing retail units, part demolition and erection of new retail warehousing at St. Nicholas Retail Park, St. Nicholas Gate, Carlisle. The 4 hectare site is located within a Large Stores and Retail Warehouses area as identified within the Proposals Map of the Carlisle District Local Plan 2001-2016.
- 5.2 The site lies to the south-west of London Road, close to the road junction of London Road and Brook Street, to the south-east of the City Centre. The site is irregular in shape with a flat topography and is currently used as a retail park with associated car parking to the frontage. It is bounded to the south-west by the Settle to Carlisle Railway which is also a designated Conservation Area; to the north-west by Woodrouffe Terrace which is a row of two storey Grade II Listed Buildings that are within the Botchergate Conservation Area; to the north-east by London Road; and to the south-east by the Railway Inn which is a vacant two storey Grade II Listed public house.
- 5.3 London Road is the main thoroughfare into the City Centre from the south and opposite the application site are several commercial premises that occupy the ground floor of the buildings and above which is residential accommodation. The area is largely dominated by two storey brick built properties under slate roofs.

Background

- 5.4 The application seeks reserved matters approval for the means of access, appearance, landscaping, layout and scale following the outline planning consent that was granted on 9th November 2006 under application reference 03/1362 following consideration of the application by Members of the Development Control Committee.
- 5.5 In summary the works illustrated within the Outline application consisted of:
1. Demolition of the unit occupied by National Tyres;
 2. Demolition of an adjacent 'stand alone' unit within the development currently occupied by Halfords (at the most southerly part of the retail park);
 3. Partial demolition of the unit at the northern end of the site which is currently occupied by MFI Ltd. to allow for a new access to the service yard from St. Nicholas Bridges to the rear. The total area of floorspace

- proposed to be demolished is 1908 sq. m. (gross);
4. The erection of a new, larger unit (3068 sq. m. gross) positioned in approximately the same location as the Halfords Unit;
 5. The erection of a small new freestanding unit close to the main site entrance (proposed 501 sq. m. gross floorspace);
 6. The erection of two new units related to the main blocks of the existing development, one a "gap" between Carpet Right and Netto (727 square metres gross); and the other at the northern end adjacent to the Woodrouffe Terrace boundary (576 square metres gross). The total floorspace represented in the above works was 4875 square metres, which represented an overall increase of 2963 square metres gross (i.e. with the demolitions of existing buildings discounted); and
 7. Rationalisation of the access and circulation arrangements through the closure of the existing secondary access from London Road. The rear access from St. Nicholas Bridge was shown to become solely a service access, with no route through the site between the two entrances.

Proposal

- 5.6 The retail park has been established for some twenty one years and was a re-development of the former Cowans Sheldon works. It presently comprises 6 units including Burger King situated close to the London Road frontage. Adjacent to the existing complex, towards the southern end of the street frontage, is a building which pre-dates the re-development and was not included within the site of that application. It was last occupied by National Tyres Ltd. This building was incorporated into the outline application and is within the current application site. It is proposed to demolish that building and redevelop its site and to undertake other works.
- 5.7 The Reserved Matters application follows the indicative drawings submitted as part of the outline planning consent in terms of layout and scale. The application proposes the demolition of two of the existing units, namely the building formerly occupied by National Tyres Ltd and the building currently occupied by Halfords, together with the partial demolition of the building formerly occupied by MFI.
- 5.8 A larger replacement building would then be constructed on the site of the Halfords building. The two storey building would project significantly forward of the existing Halford's building and extend close to the site boundary with London Road. Its principal elevation would be relocated to would face north-west i.e. into the retail park. The applicants consider that an active frontage would be formed facing London Road. The building would be constructed from flat and profile metal sheeting together with curtain wall glazing and would incorporate a flat roof.

- 5.9 A new retail unit would be constructed between the buildings currently occupied by Carpet Right and Netto. A further retail unit would be constructed adjacent to the north-west elevation of the former MFI building, close to the boundary with Woodrouffe Terrace. The former National Tyres building would be demolished and a replacement building constructed as a frontage building to London Road but relocated to the north-west. These new buildings would be constructed from materials that would match those proposed on the new unit that would be formed on the current Halfords site.
- 5.10 The existing vehicular entrance in the south-east corner close to Halfords would be closed. There would only be one vehicular entrance that would serve the site and that would be the existing one located centrally in the retail park's frontage that leads onto London Road. This is a staggered junction opposite Brook Street and as part of the outline application, a S106 Agreement was signed that provides finances to upgrade this junction to a signalised junction. The vehicular entrance to the rear leading on to St. Nicholas Bridges would remain but would only be accessible for delivery vehicles accessing the service area to the rear.
- 5.11 The parking area within the site would be upgraded and remodelled and the development would include additional landscaping. A total of 320 parking spaces would be provided including 22 spaces for disabled persons along with 5 spaces for motorcycle provision.
- 5.12 All of the foregoing is in accordance with the indicative drawings submitted and approved as part of the outline application. As part of that application, the developers showed that the existing buildings would remain unaltered; however, as part of the current proposal the submitted drawings illustrate that these buildings would be refurbished and re-clad using materials consistent with the proposed new units.
- 5.13 The relevant planning policies against which the application is required to be assessed are Policies DP1, DP3, DP5, DP7, W5 and CNL2 of the North West Regional Spatial Strategy to 2021 and Policies DP1, DP2, CP3, CP5, CP6, CP9, CP15, CP16, CP17, EC5, EC8, LE12, LE19 and T1 of the Carlisle District Local Plan 2001-2016. It is considered that there are eight main planning issues raised by this proposal.

Assessment

1. The Principle Of Development

- 5.14 The site is within a Large Stores and Retail Warehouses area and Policy EC5 of the Carlisle District Local Plan 2001-2016 is appropriate. This Policy states that proposals for large stores and retail warehouses with large adjacent customer car parks will not be permitted except on sites allocated in this Plan. Furthermore, Policy DP1 of the Local Plan requires that development proposals should enhance the overall quality of life within Cumbria through the promotion of sustainable development that seeks to protect the environment, ensure prudent use of resources and maintain social progress and economic growth.

- 5.15 The supporting Planning Statement discusses in more detail the benefits of redeveloping the site. It is stated that the site is significantly under performing which is characterised by approximately 64% of retail floorspace being vacant. Since the outline consent was granted, two of the larger tenants on the site, MFI and Focus DIY have ceased trading and these units have been vacant for some time.
- 5.16 Condition 11 of the outline planning permission restricted the overall net increase of floor area and stated that this should not exceed 2963 square metres. The reserved matters application is compliant with this requirement.
- 5.17 The site is well related to the existing highway network and is in a location that is accessible by other modes of transport, including public transport. Specific on-site parking provision will be created which is acceptable and the principle of development does not conflict with current policy guidance.
- 5.18 To summarise, the proposal will result in net increase of 2963m² (around 32,000 ft²) of additional floorspace but this would all be for the sale of bulky goods. The remaining floorspace in the Retail Park will also be subject to the existing condition restricting sales to bulky goods, with the exception of the exemptions previously granted (e.g. Netto and Brantanos (shoes)). The amended proposal is thus considered acceptable in terms of retail planning policy.

2. Scale And Design

- 5.19 The retail park predominantly comprises a series of large retail units that face London Road but which are set back some distance from the frontage interspersed by an abundance of car parking. Within this area are two detached buildings; one is currently vacant (but was last occupied by National Tyres Ltd) while the other operates as a Burger King Drive Thru. The main row of buildings are two storeys in height and of dark brown clay facing brick construction with buff brick detailing. Some of the buildings have large two storey entrance canopies supported on brick piers constructed of matching, decorative brickwork detailing.
- 5.20 Adopted policies generally require that development is appropriate, in terms of quality, to that of the surrounding area. Proposals should, therefore, incorporate high standards of design including care in relation to siting, scale, use of materials and landscaping that respects and, where possible, enhances the distinctive character of townscape and landscape. This is reflected in Policy CP5 of the Local Plan which requires that development proposals should also harmonise with the surrounding buildings respecting their form in relation to height, scale and massing and making use of appropriate materials and detailing.

- 5.21 As previously stated, there is a range of types and scale of buildings in the locality but most notable and most dominant are the two storey brick built terraced residential properties along Brook Street, Oswald Street, Garden Street, Woodrouffe Terrace, St Nicholas Street and Lindisfarne Street. The buildings on London Road, opposite the application site, are slightly larger in scale, being three storeys. The applicant has submitted the application on the basis of the illustrative drawings that formed part of the outline application. The intention to demolish the National Tyres building, which detracts from the character of the area, would enable the reconstruction of a replacement building closer to the proposed vehicular entrance. In conjunction with the existing Burger King building, the applicant states that these buildings would form a presence and a 'gateway' to the retail park.
- 5.22 The applicant states that the height of the proposed buildings would be between 8 and 10 metres which is similar to the existing buildings. The most prominent building would be Unit 1 which would be constructed in the south-east corner of the site. Due to the level of the site and the adjacent pavement, this building would appear to be 6.3 metres in height when viewed in conjunction with the Railway Inn public house whereas the true height of the building would be 9.3 metres. The boundary wall slopes down along London Road and would serve to reduce the overall massing of the building.
- 5.23 The proposed development would involve a suite of materials that would result in a contemporary appearance. The outline application sought to retain the brickwork on the existing buildings with modern materials used on the proposed additional development. The current application, however, departs from that approach and seeks to also re-face the existing buildings which the applicant states would provide an holistic and comprehensive, contemporary development that would promote the occupancy of the buildings and subsequently contribute to the regeneration of the site and the local economy.
- 5.24 The proposed buildings would be of a scale and design that is appropriate to modern retail park development; however, the site is within an inner-urban location, not a peripheral retail park. The existing buildings were designed to reflect the character and appearance of the surrounding buildings and to be sympathetic with the historic constraints and urban grain. Whilst the overall appearance of the site as proposed would, in its own terms, be distinctive in character and appearance, involving a contemporary design that would incorporate modern materials that would assist in promoting the energy efficiency of the buildings, the scale and use of those materials bear no resemblance to the character or appearance of the area generally. The site is prominently located on a principal, arterial approach to the City and consequently, the development would be highly visible with the architectural materials now proposed being obtrusive and detrimental to the character of the area.

3. Impact On The Character And Appearance Of The Conservation Area And Adjacent Listed Buildings

- 5.25 The Railway Inn is Grade II Listed; the St. Nicholas Arms public house to the north of the application site on the opposite side of London Road is Grade II Listed; and the properties along Woodrouffe Terrace are also Grade II Listed as is the terrace of properties at St Nicholas Street beyond. To the rear of the site is the Settle to Carlisle Conservation Area and to the north lies the Botchergate Conservation Area. The designated Conservation Areas and the Listed Buildings are important features of the City's historic environment and proper consideration should be paid to the potential impact of any development in accordance with planning policy requirements. The site is, effectively, book-ended by Listed Buildings.
- 5.26 Members will note from the consultation responses that have been received that the Council's Urban Designer is broadly happy with the proposal although, surprisingly, his comments focus on the landscaping and pedestrian access arrangements with no comments on the architectural quality or appearance of the development. The Conservation Area Advisory Committee (CAAC) does, however, address those aspects and strongly opposes the development on two issues. Firstly, it is considered that the proposed scheme is of a poor design, that the development would adversely affect the Settle to Carlisle Conservation Area and would provide an inappropriate impression to persons visiting the City.
- 5.27 The second issue raised by CAAC is that an *"opportunity to improve the London Road route into Carlisle is also being lost"* and that *"the new units replacing Halfords and the Tyre Distributor consisted of flat, bland elevations that created a negative impact and are no better than the existing and some elements are actually worse"*. In considering these views, it is evident that the principle of the increase in floorspace of the buildings has been accepted through the assessment of the retail capacity and the granting of the outline planning permission. The Design and Access Statement identifies that the scale of the buildings would be offset by the site levels and the use of materials to provide variation and visual interest in the London Road elevations; however, in the view of the CAAC, the combination of the scale and the use of materials in both the new and existing buildings would give rise to a development that would detract from the character of the designated Conservation Areas.
- 5.28 The Railway Inn is located immediately adjacent to the south-east boundary of the site. The proposed development would be closer to the frontage with London Road and would have a visual impact on its setting and appearance; however, it is not uncommon to have contemporary buildings in close proximity to Listed Buildings. It would be impractical, and arguably unreasonable, to expect the new build to reflect the design and materials used in the Listed Building, particularly given the modern appearance of the existing Halfords store.
- 5.29 Woodrouffe Terrace is situated to the north-west of the application site. The terrace is separated by a 2m high brick wall and the single width highway beyond that serves the properties. The impact of development in this part of the retail park would be less severe with an extension being constructed to the gable of the former MFI building. Given the distance between the

application site and these properties along with the intervening physical barrier, it is not considered that there development would adversely affect the character or setting of these Listed Buildings. Members will note that no objection has been raised from any consultees in respect of the impact on the Listed Buildings.

4. Effect On The Living Conditions Of The Occupiers Of Neighbouring Properties

- 5.30 Planning policies require that development proposals do not adversely affect the living conditions of occupiers of residential properties by virtue of inappropriate development, scale or visual intrusiveness. The nearest residential properties are on the eastern side of London Road above the existing shops and to the north-west in Woodrouffe Terrace which are, respectively, 22.5 metres and 27 metres from any new development. The proposal is therefore compliant with the Council's practice of securing a minimum distance of 12 metres between windows in habitable rooms and blank walls that is detailed in the draft Supplementary Planning Document "Achieving Well Designed Housing".
- 5.31 The proposed new buildings are appropriate to the overall scale of buildings within the locality. In this respect it is not considered that the proposal would be disproportionate or obtrusive and it is not considered that the occupiers of the adjacent buildings would suffer from an unreasonable loss of daylight or sunlight.

5. Traffic Issues

- 5.32 Policy T1 of the Local Plan requires that there is sufficient parking provision within the site for the relevant development. This proposal seeks to provide an overall level of 320 spaces including 22 spaces for disabled persons and 5 spaces for motorcycles.
- 5.33 The proposal seeks to utilise only one vehicular access to serve the site opposite Brook Street. This is presently a congested and difficult junction due to the volumes of traffic travelling in both directions along London Road; the presence of a filter lane from the north and south on London Road; the number of vehicles trying to cross London Road from Brook Street into the application site and visa versa; and the staggered nature of the junction. Condition 3 of the outline planning consent stipulates that works should not commence until the main junction has been signalised. These details are formalised in a S278 Agreement with the Highway Authority.
- 5.34 The closure of the remaining vehicular entrances would reduce the vehicle movements through the site from London Road to St. Nicholas Bridges and, hence, removes the opportunity for rat-runs. The enhancements to the remaining junction would significantly improve the traffic arrangements at this location on London Road.

5.35 Given the surrounding road network, the bus routes and services near to the site, and the anticipated number of proposed vehicle movements, the Highway Authority has raised no objection subject to the imposition of conditions. One of these is a requirement for the applicant to submit a Travel Plan. This current application seeks reserved matters consent. No such condition was imposed with the outline planning permission and it would be unreasonable to impose such a condition as part of any consent for an application which essentially seeks to discharge the original conditions.

6. Public Access

5.36 The Design and Access Statement makes reference to the need for the design and layout of the buildings to meet the highest standards of accessibility and inclusion for all potential users regardless of disability, age or gender. This accords with the objectives of Policy CP15 of the Local Plan. Additional drawings are awaited at the time of writing this report showing the internal layout and consequently comments are also awaited from the Council's Access Officer.

7. Drainage Issues/Green Design

5.37 The design of the buildings would take into account energy efficiency and this would be achieved through the use of thermal efficient building materials, the installation of energy efficient boilers and heating systems, and the installation of energy efficient lighting systems. A portion of the water from the roof area would also be collected by a rainwater harvesting facility and re-used for irrigation purposes.

5.38 It is the applicant's intention to improve air leakage from the building and utilise low carbon technology, thereby reducing the overall carbon footprint of the development. In this respect, the proposal would be compliant with the objectives of Policies CP9 and CP10.

8. Crime Reduction

5.39 The need for this facility can be considered as a material consideration. Section 17 of the Crime and Disorder Act 1998 promotes the practice of partnership working and states:

"Without prejudice to any other obligations imposed upon it, it shall be the duty of each authority to exercise its various functions with due regard to the likely effect of the exercise of those functions on, and the need to do all it reasonably can to prevent crime and disorder in its area."

5.40 It is acknowledged that fear of crime can form the basis of a reason for refusal and in this regard Section 17 is relevant.

5.41 Cumbria Constabulary has raised no objection in principle to the development. The outline approval was subject to a S106 Agreement that secured funding for additional CCTV coverage of the site which is at present, a monitoring shortcoming. The supporting documents submitted by the

application refer to a camera and a system of cameras and clarification is being sought as to what is actually being proposed.

- 5.42 The provision of cycle storage, improvements to the rear boundary and the creation of secure access to the service area to the rear, which is presently unrestricted, is welcomed in principle by Cumbria Constabulary. Specific details that relate to the type of boundary fencing and the use of Sheffield cycle stands that have been suggested for incorporation in the scheme have been relayed to the applicant. At the time of writing this report, no response has been received.

Conclusion

- 5.43 In overall terms, the site is highly prominent on one of the major approaches to the City and issues involved in the consideration of this proposal are balanced between the need for investment to regenerate its performance as Retail Park while ensuring that the design and materials used reflect the site's location buffered by features of the city's heritage. The retail park has been in existence for some time and the applicant's argue that its appearance reflects this. The buildings are currently largely vacant and underused and it is argued that a modern development of contemporary appearance would act as a catalyst to attract new business into the retail park. There would, clearly, be undoubted benefits to the surrounding traffic network through the closure of two of the existing vehicular accesses and the improvement of the junction with London Road through the facilitation of a signalised junction. The site would also benefit from greater security features and improved landscaping. The scale of the buildings would be appropriate and the living conditions of the occupiers of the neighbouring properties would not be adversely affected by the development.
- 5.44 In considering these proposals, it is evident that the wholesale use of modern materials to face both the existing and proposed buildings would bear no resemblance to the predominantly brick and slate constructed buildings in the immediate vicinity of the site. The development would result in much more strident and obtrusive (albeit "contemporary") buildings that would adversely affect the appearance of the streetscene and the designated Conservation Areas. The important aesthetic relationship that the existing brick built buildings have with the surrounding area, notably terraced buildings to the north and east, would be lost as a result of the proposal and, in this respect, the development would take on the appearance of a development more typical of a 'peripheral' retail park rather than respecting and complementing a sensitive site in an 'inner-urban' location. As such, it is considered that the benefits of the scheme do not outweigh the visual disbenefits to the character and appearance of the area and for this reason, it is recommended that the application is refused.
- 5.45 Through discussions with Officers the applicant has indicated that the scheme may be revised to take account of these concerns about the development. It is anticipated that these will be received prior to the Committee meeting and, should this be the case, any amended details would be made available for Members further consideration.

6. Human Rights Act 1998

6.1 Several provisions of the above Act can have implications in relation to the consideration of planning proposals, the most notable being:

Article 6 bestowing the "Right to a Fair Trial" is applicable to both applicants seeking to develop or use land or property and those whose interests may be affected by such proposals;

Article 7 provides that there shall be "No Punishment Without Law" and may be applicable in respect of enforcement proceedings taken by the Authority to regularise any breach of planning control;

Article 8 recognises the "Right To Respect for Private and Family Life";

6.2 Article 1 of Protocol 1 relates to the "Protection of Property" and bestows the right for the peaceful enjoyment of possessions. This right, however, does not impair the right to enforce the law if this is necessary, proportionate and there is social need;

6.3 Article 8 and Article 1 Protocol 1 are relevant but the impact of the development in these respects will be minimal and the separate rights of the individuals under this legislation will not be prejudiced. If it was to be alleged that there was conflict it is considered not to be significant enough to warrant the refusal of permission.

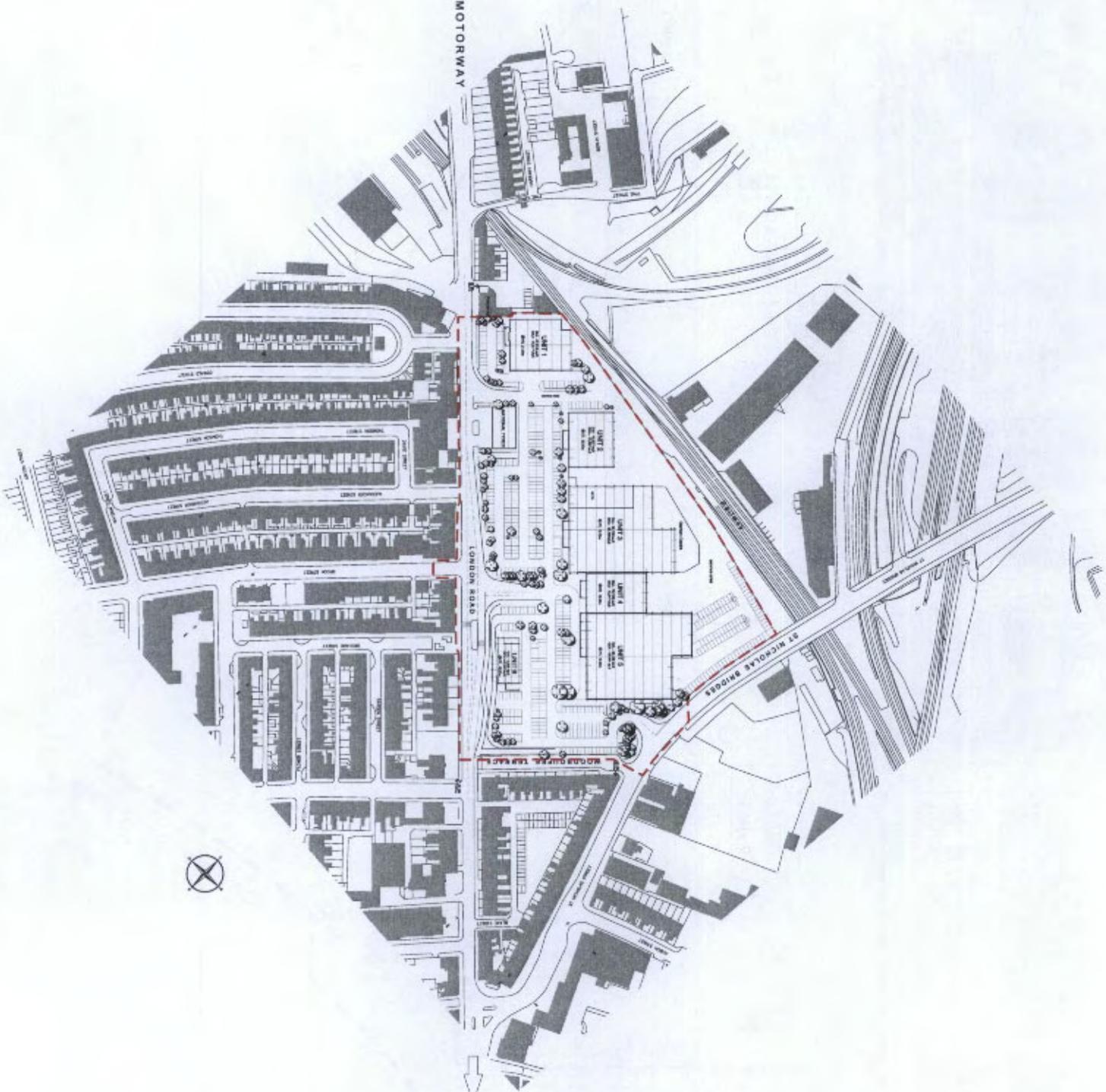
7. Recommendation - Refuse Permission

1. **Reason:** The proposal involves the erection of 4no. additional commercial buildings on a prominently located retail park on the gateway to the City from the south. The land is bordered by the Settle to Carlisle Conservation Area to the west and the Botchergate Conservation Area to the north with Grade II Listed Buildings adjacent to the proposed development. In such circumstances the proposed development, by reason of the design and use of materials would have a dominant impact resulting in an obtrusive development that would be detrimental to the character of the streetscene and the adjacent Conservation Areas. The use of incongruous materials has little regard to the use of traditional brick and slate materials used in the surrounding buildings. The development would constitute a 'peripheral' retail park appearance in an 'inner-urban' location and the economic and highway benefits of the scheme would not outweigh the visual disbenefits to the character and appearance of the area. For this reason and in this context, the proposal is considered to be contrary to

criterion 1 and 3 of Policy CP5 (Design); criterion 2 and 4 of Policy CP6 (Residential Amenity); and criterion 1 and 4 of Policy LE19 (Conservation Areas) of the Carlisle District Local Plan 2001-2016.

FROM J42 M6 MOTORWAY

TO CITY CENTRE



CLIENT:
POSTFUND CUSTOOM TRUSTS LIMITED

PROJECT:
ST NICHOLAS GATE SERIAL PARK CARSLILE

DRAWING TITLE:
EXISTING LOCATION PLAN

DATE: 04/08

SCALE: 1:200

PROJECT NO.: 7988

DATE: 01/01

DESIGNER: []

ARCHITECT: []

ENGINEER: []

PLANNING: []

LANDSCAPE: []

STRUCTURE: []

MECHANICAL/ELECTRICAL/PLUMBING: []

HAZARDOUS WASTE: []

ENVIRONMENTAL: []

ARCHITECTURE: []

MECHANICAL/ELECTRICAL/PLUMBING: []

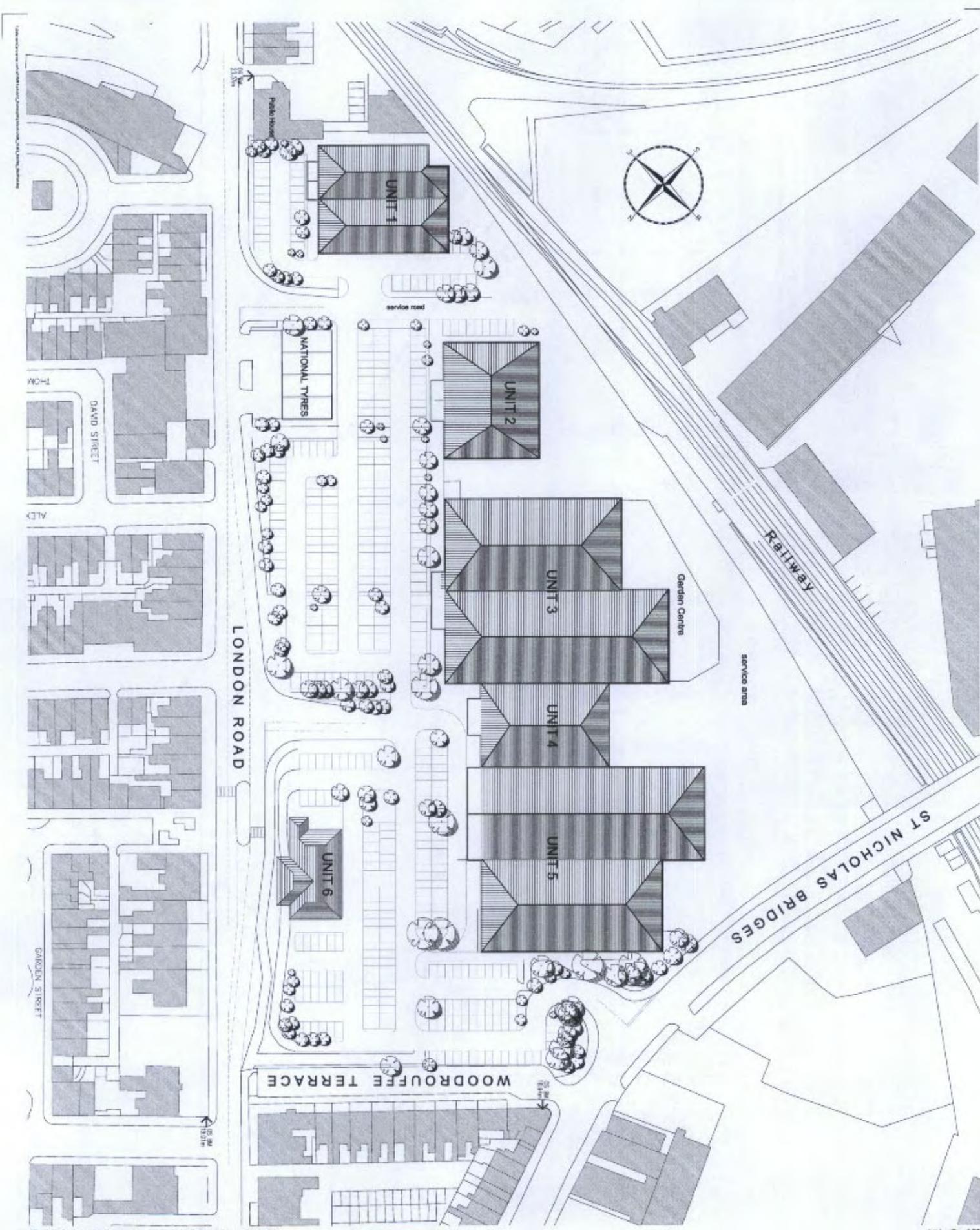
HAZARDOUS WASTE: []

ENVIRONMENTAL: []

PREPARED BY: []

CHECKED BY: []

DATE: []



CLIENT:
 ST NICHOLAS GATE
 RESIDENTIAL CUSTOMER
 TRUSTEES LIMITED

PROJECT:
 ST NICHOLAS GATE
 SEVEN PARK
 CARIBBE

DRAWING TITLE:
 EXISTING
 ROOF PLAN

DATE: 15/03/09

SCALE: 1:500

PROJECT NO.: 7
FL.: 003

ARCHITECT:
 ST NICHOLAS GATE
 SEVEN PARK
 CARIBBE

New Building
 Conversion
 Extension
 Refurbishment
 Other

Full Planning
 Partial Planning
 Other

New
 Existing

This drawing is the property of the Architect and is not to be used for any other purpose without the written consent of the Architect.

The Architect's liability is limited to the professional services provided by him/her.

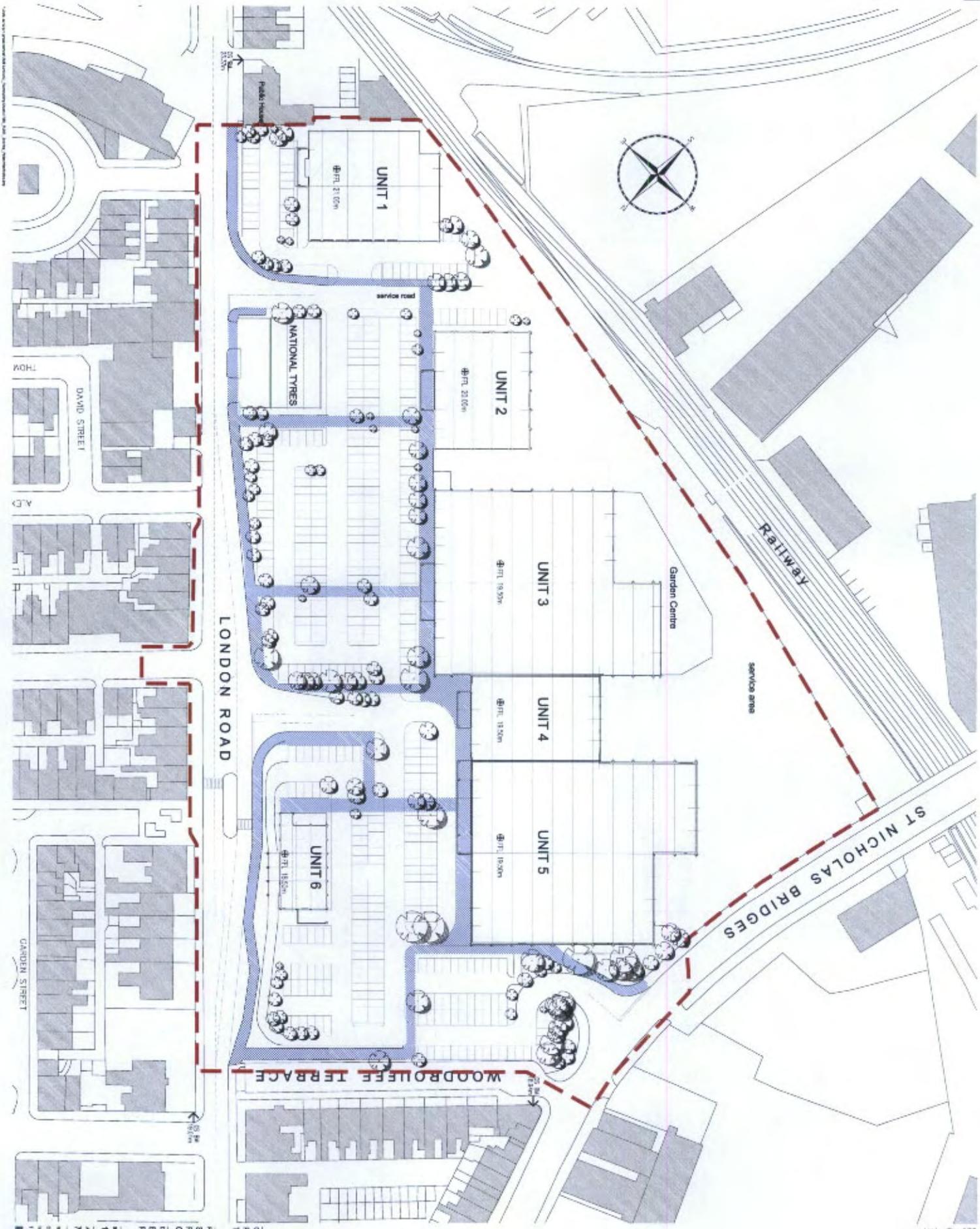
Date: 15/03/09

Scale: 1:500

Project No: 7

Floor: 003

Architect: ST NICHOLAS GATE SEVEN PARK CARIBBE



Project: ST NICHOLAS GATE
 Client: ROSSIGNOL CUSTOM HOMES LIMITED
 Date: 11/04/11

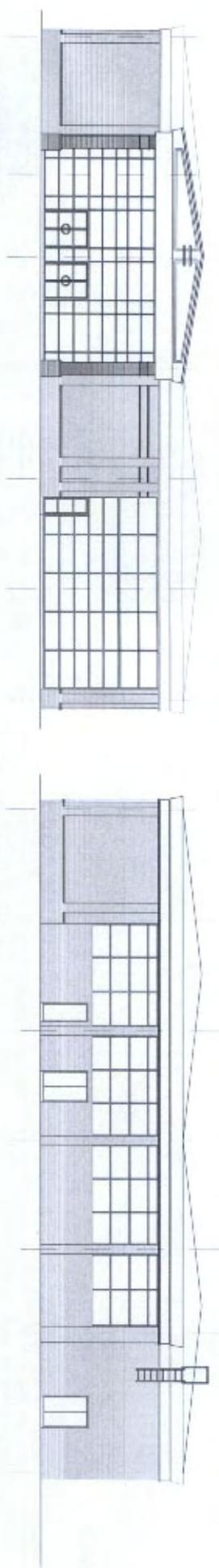
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 Client: ROSSIGNOL CUSTOM HOMES LIMITED
 Date: 11/04/11

Architect: [Name]
 Date: [Date]

Scale: 1:1000

North Arrow

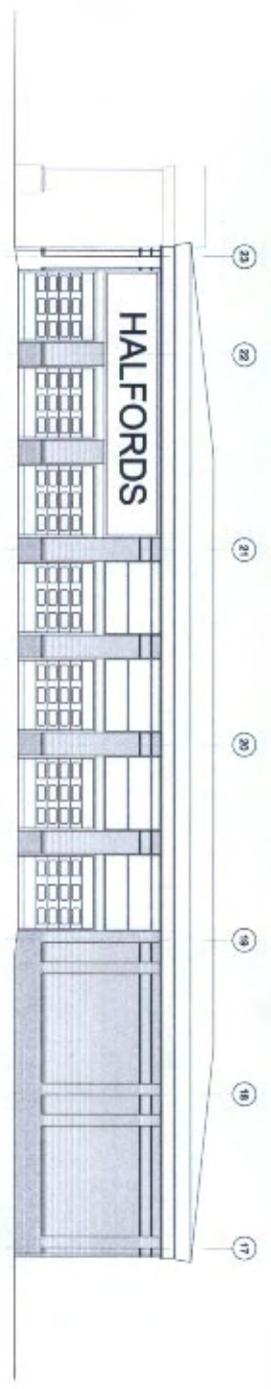
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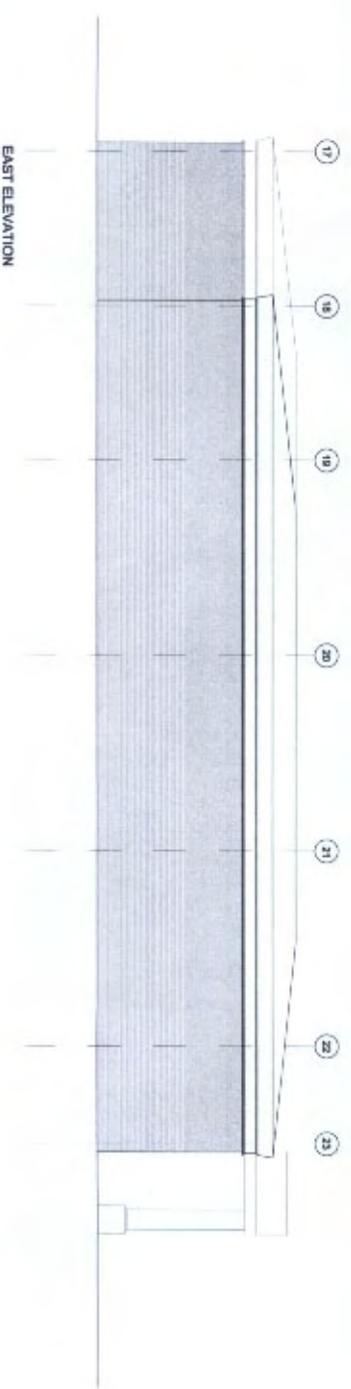
LONDON ROAD ELEVATION

RAILWAY ELEVATION

Project: Halfords Retail Park, St. Nicholas Gate, Retal Park, Carlisle
 Drawing No: 20/10/2018
 Date: 20/10/2018
 Scale: 1/200
 Author: [Name]
 Checker: [Name]
 Date: 20/10/2018



WEST ELEVATION



EAST ELEVATION

CLIENT:
 ROSSER AND CUSTONIAN
 TRUSTEES LIMITED

PROJECT:
 ST NICHOLAS GATE
 RETAL PARK
 CARLISLE

DATE:
 20/10/2018

SCALE:
 1/200

DATE:
 20/10/2018

PROJECT:
 ST NICHOLAS GATE
 RETAL PARK
 CARLISLE

DATE:
 20/10/2018

SCALE:
 1/200

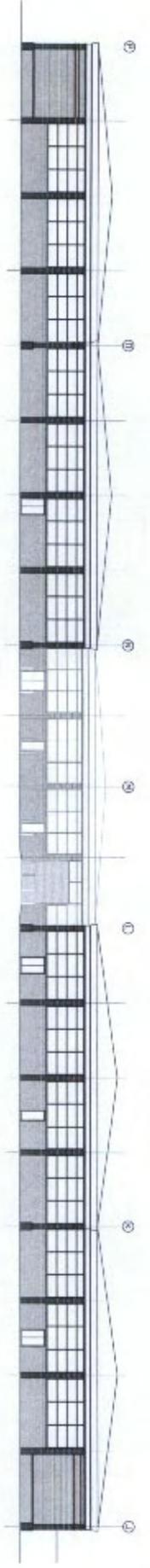
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 20/10/2018

PROJECT:
 ST NICHOLAS GATE
 RETAL PARK
 CARLISLE

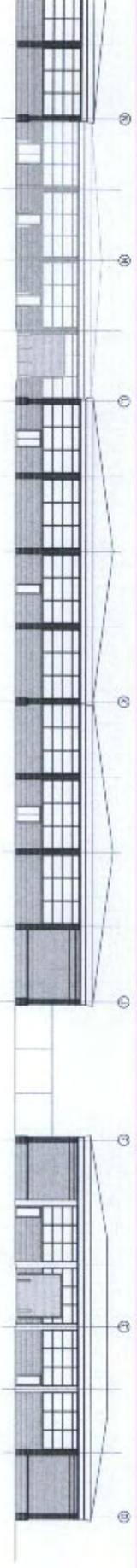
DATE:
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SCALE:
 1/200

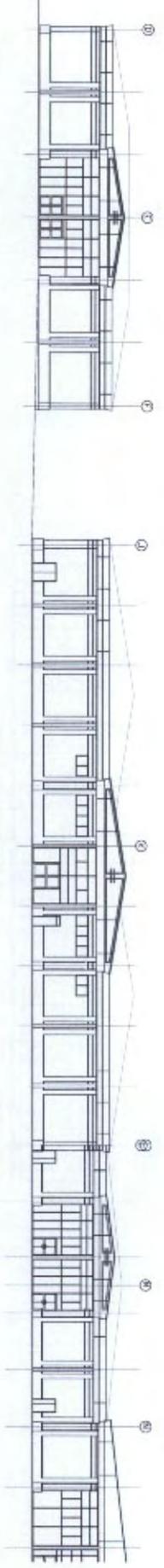
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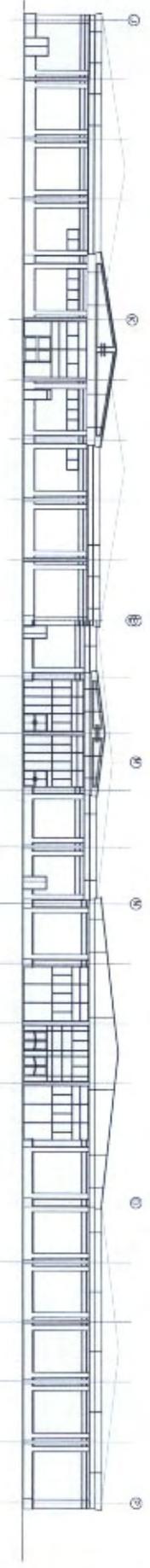
EXISTING REAR ELEVATIONS TO UNITS 3, 4 & 5



EXISTING REAR ELEVATIONS TO UNITS 3, 4 & 5



EXISTING FRONT ELEVATIONS TO UNITS 3, 4 & 5



EXISTING FRONT ELEVATIONS TO UNITS 3, 4 & 5

Client:
 ROSSLAND CUSTOM
 TRUSTEES LIMITED

Project:
 ST NICHOLAS GATE
 ST NICHOLAS PARK
 CASTLE

Site Ref:
 ST NICHOLAS GATE
 ST NICHOLAS PARK
 CASTLE

Scale:
 1:200

Date:
 01/01/2023

Author:
 J. Doe

Reviewer:
 K. Smith

Project No:
 2023-01-01

Sheet No:
 01/01

Scale:
 1:200

Date:
 01/01/2023

Author:
 J. Doe

Reviewer:
 K. Smith

Project No:
 2023-01-01

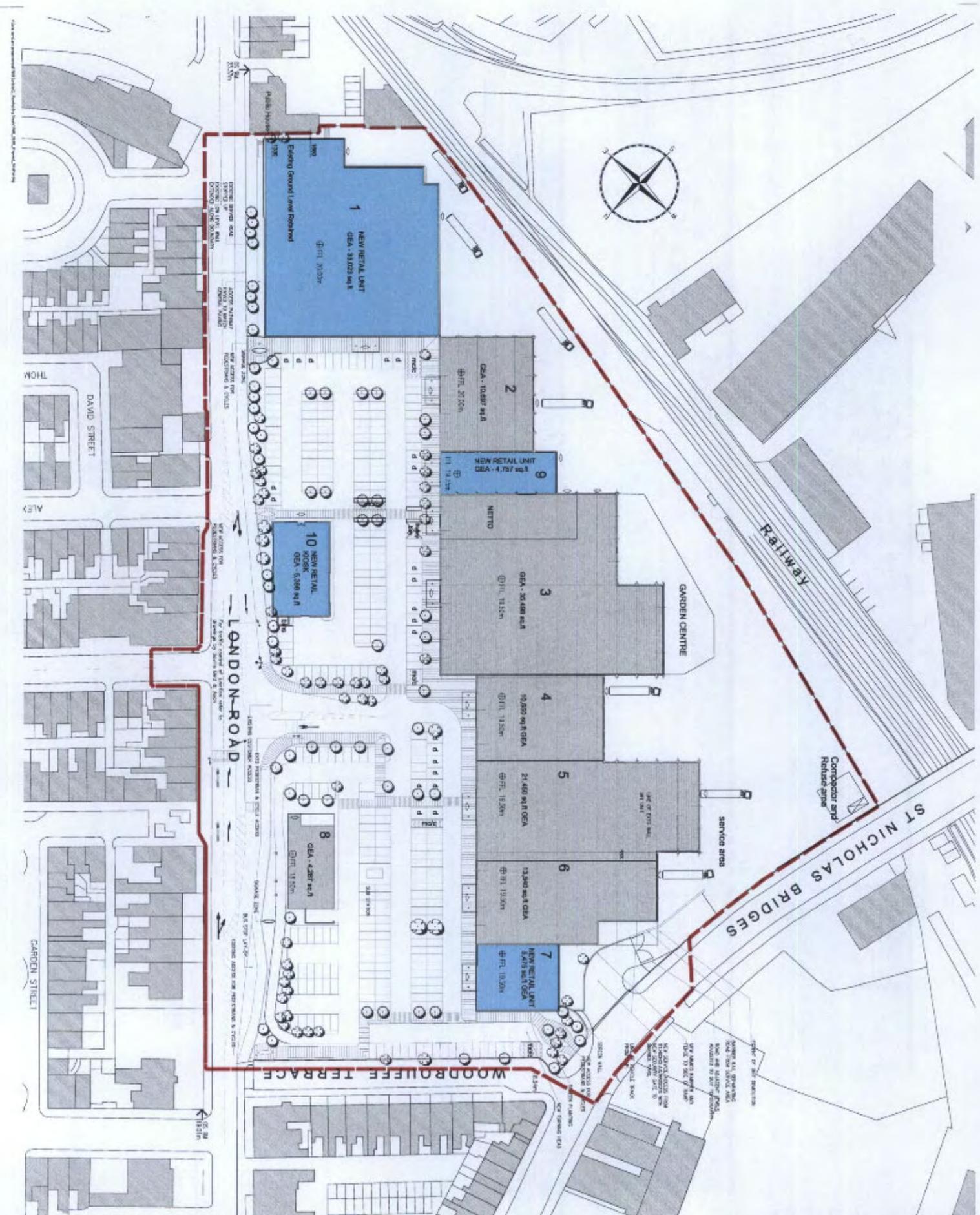
Sheet No:
 01/01

Scale:
 1:200

Date:
 01/01/2023

Author:
 J. Doe

Reviewer:
 K. Smith



Proposed Customer Car Parking
300 Spaces including 22 disabled spaces
Plus 5 motorcycle and cycle parking zones (inc/2)

Proposed New Schedule

- Unit 1 13,023 sq ft / 3068 sqm
- Unit 2 10,597 sq ft / 994 sqm
- Unit 3 36,488 sq ft / 3286 sqm
- Unit 4 10,550 sq ft / 980 sqm
- Unit 5 21,460 sq ft / 1993 sqm
- Unit 6 13,248 sq ft / 1258 sqm
- Unit 7 3,675 sq ft / 509 sqm
- Unit 8 4,757 sq ft / 388 sqm
- Unit 9 4,757 sq ft / 442 sqm
- Unit 10 5,298 sq ft / 507 sqm

Total proposed area
144,575 sq ft / 13,441 sqm
Total existing area
115,191 sq ft / 10,702 sqm
Additional area created
29,484 sq ft / 2739 sqm

Client:
POSSIFIND CUSTOMER
TRUSTEES LIMITED

Project:
ST NICHOLAS GATE
RETAIL PARK
CAROUSE

Drawn Title:
PROPOSED
SITE PLAN

Date: 1508
Scale: 1:100
Author: P. 011
Check: P. 011
Date: 02/10

Scale: 1:100
Author: P. 011
Check: P. 011
Date: 02/10



UNIT 1 - LONDON ROAD ELEVATION

UNIT 2

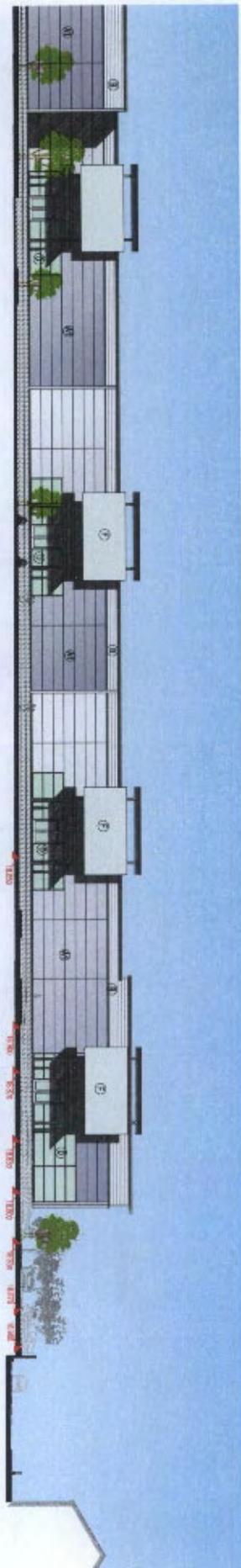
UNIT 9

UNIT 3

WATERMARK KEY TO ELEVATIONS



- 10 Composite window sliding panels
- 11 Composite window sliding panels (containing Casery)
- 12 Hybrid metal cladding
- 13 FRP metal cladding
- 14 FRP metal cladding
- 15 Hybrid metal bulk-up cladding
- 16 New signage features
- 17 Existing balconies
- 18 Existing window Cladding



UNIT 3

UNIT 4

UNIT 5

UNIT 6

UNIT 7

CAR PARK WOODROUFFE TERRACE



LONDON ROAD

UNIT 1

UNIT 2

SERVICE YARD

Client: POSSESS AND CUSTOMER TRUSTEES LIMITED

Project: ST NICHOLAS GATE RETAIL PARK CHALISLE

Architect: BFC

Project No: 20/0004/0001

Site No: 001/23

Scale: 1:200

Date: 04/23

Author: P. SIE

Check: P. SIE

Drawn: P. SIE

Scale: 1:200

Date: 04/23

Project No: 20/0004/0001

Site No: 001/23

Scale: 1:200

Date: 04/23

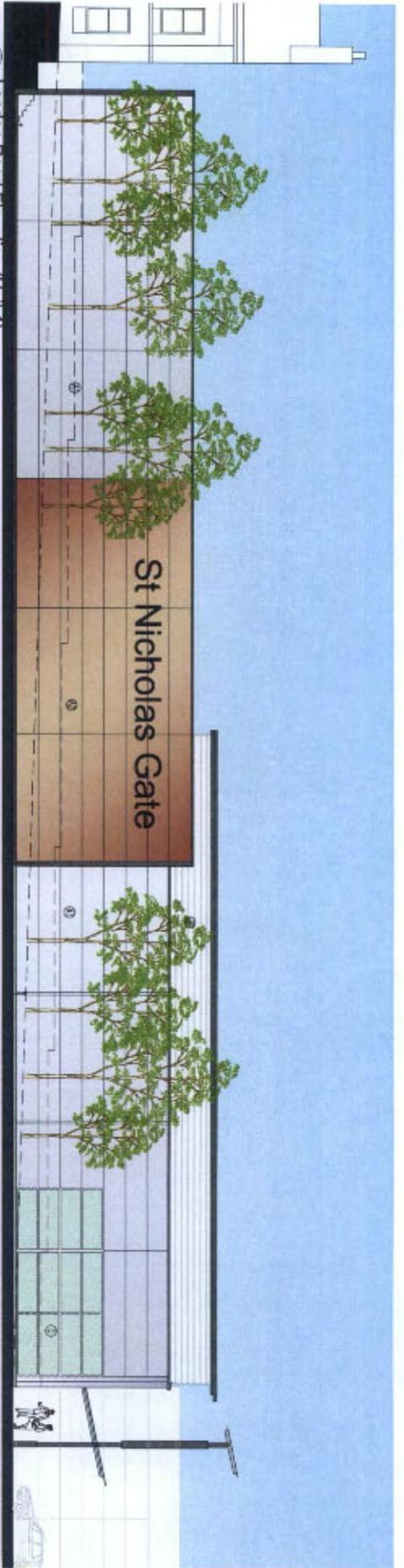
Author: P. SIE

Check: P. SIE

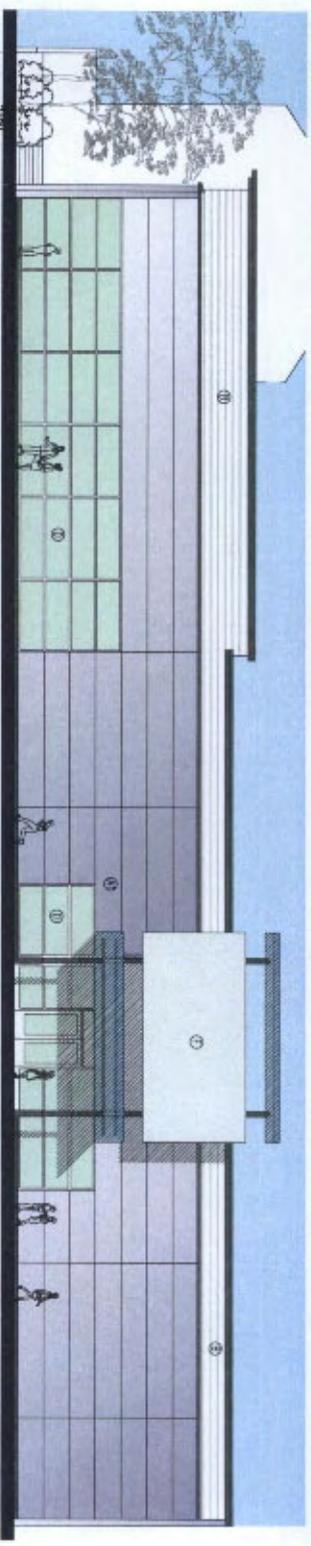
Drawn: P. SIE

Scale: 1:200

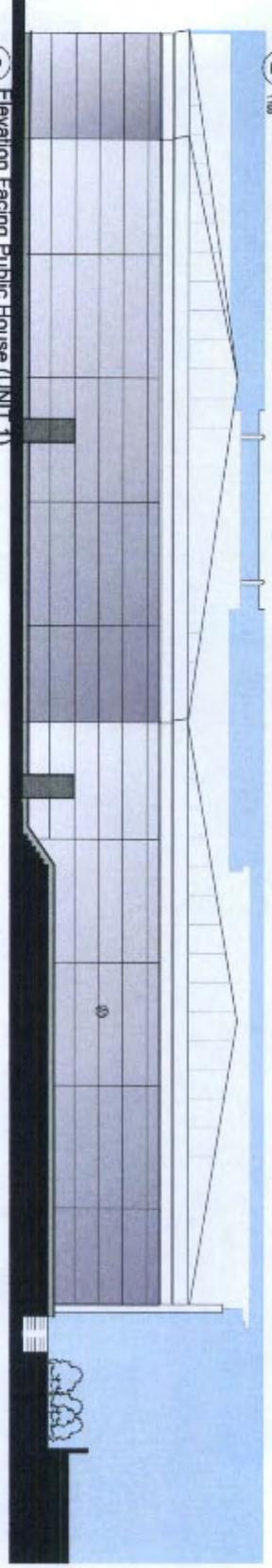
Date: 04/23



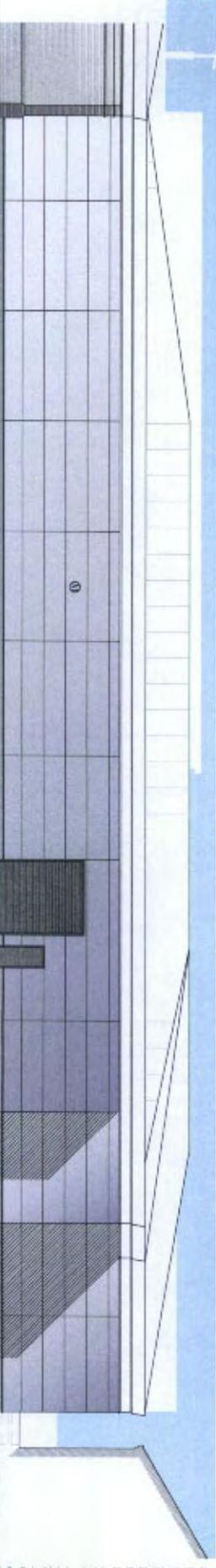
1 London Road Elevation (Unit 1)



2 Car Park Elevation (Unit 1)



3 Elevation Facing Public House (UNIT 1)



4 Service Yard Elevation



① Complete window detailing panels
 ② Complete window detailing panels (including detail)
 ③ Window frame detailing
 ④ Metal roof detailing
 ⑤ Cladding wall panels
 ⑥ Archival metal wall detailing
 ⑦ New signage markers
 ⑧ Existing landscape
 ⑨ Existing stone's cladding

UNRENDERED TO BALANCE
 ① Complete window detailing panels
 ② Complete window detailing panels (including detail)
 ③ Window frame detailing
 ④ Metal roof detailing
 ⑤ Cladding wall panels
 ⑥ Archival metal wall detailing
 ⑦ New signage markers
 ⑧ Existing landscape
 ⑨ Existing stone's cladding

EXHIBITS BY DATE

CLIENT:
 ROSEBUD DISTONIAN
 THURTELL LIMITED

PROJECT:
 ST. NICHOLAS GATE
 RETAIL PARK
 CARLEISLE

Project Title:
 PROPOSED
 ELEVATIONS
 NEW UNIT 1

Drawn by: [Name]
Checked by: [Name]
Approved by: [Name]

Date: 11/20
Scale: 1:100

Revision: 1
Date: 11/20

Author: [Name]
Checker: [Name]
Approver: [Name]

Scale: 1:100
Date: 11/20

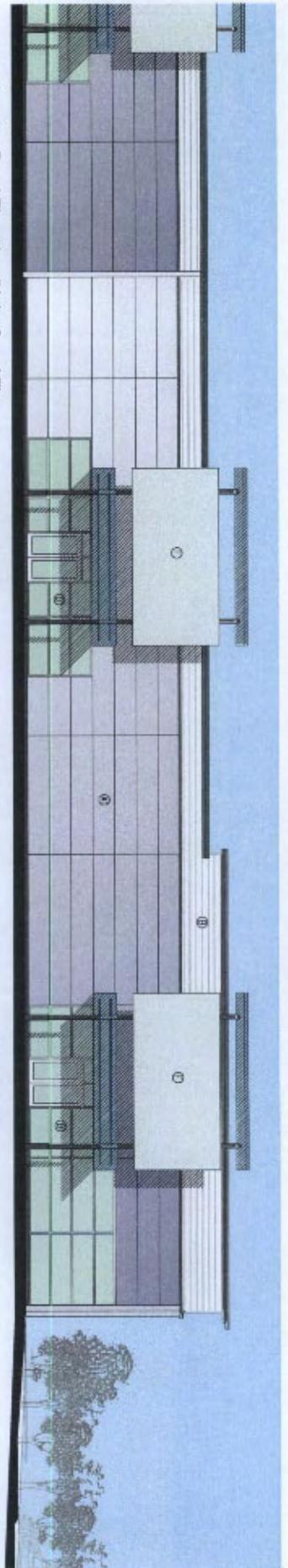
Project Title:
 PROPOSED
 ELEVATIONS
 NEW UNIT 1

Drawn by: [Name]
Checked by: [Name]
Approved by: [Name]

Date: 11/20
Scale: 1:100

Author: [Name]
Checker: [Name]
Approver: [Name]

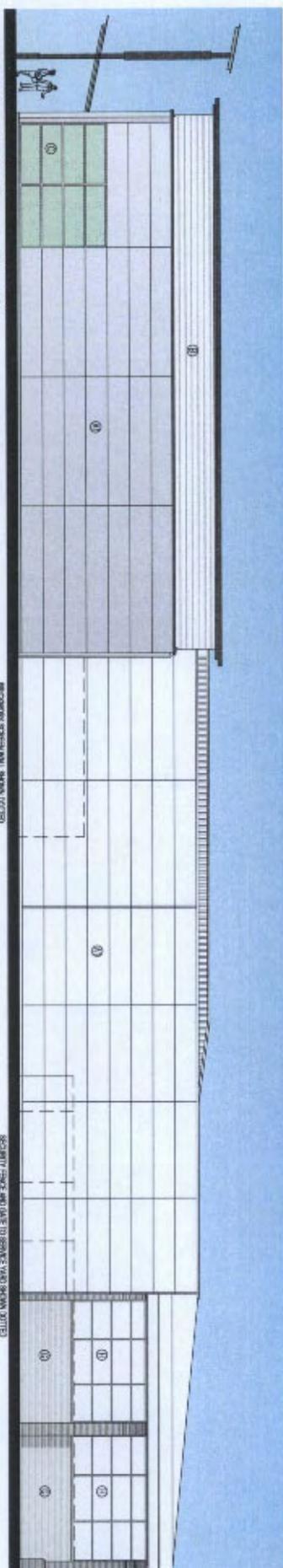
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Date: 11/20



1 London Road Elevation (Units 6 and 7)

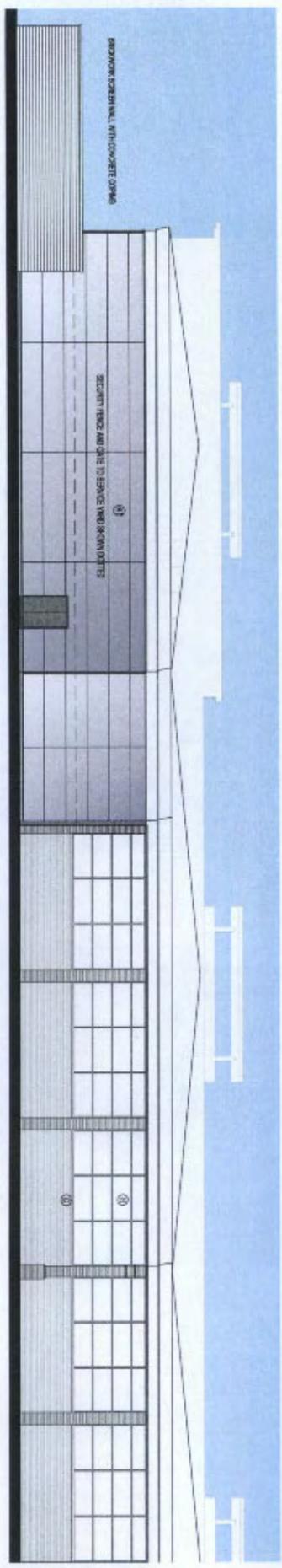
UNIT 6

UNIT 7



2 Woodruffe Terrace Elevation

UNIT 7



3 Service Yard Elevation

UNIT 6

UNIT 5

The client and the contractor shall be responsible for the accuracy of the information provided to the architect. The architect shall not be responsible for the accuracy of the information provided to the client or the contractor.

NOTES TO ELEVATIONS

- 1) Complete including shading areas
- 2) Complete including shading areas
- 3) Complete shading
- 4) Material finish shading
- 5) Material shading
- 6) Material shading
- 7) Material shading
- 8) Material shading
- 9) Material shading
- 10) Material shading

PROJECT INFORMATION

CLIENT:
 FOSBROOK CARBONUM
 TRUSTEES LIMITED

PROJECT:
 ST. NICHOLAS GATE
 RETAIL PARK
 CARLSLE

ARCHITECT:
 J. B. B. ARCHITECTS
 100, 101, 102, 103
 104, 105, 106, 107, 108
 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120

DATE:
 11/10/11

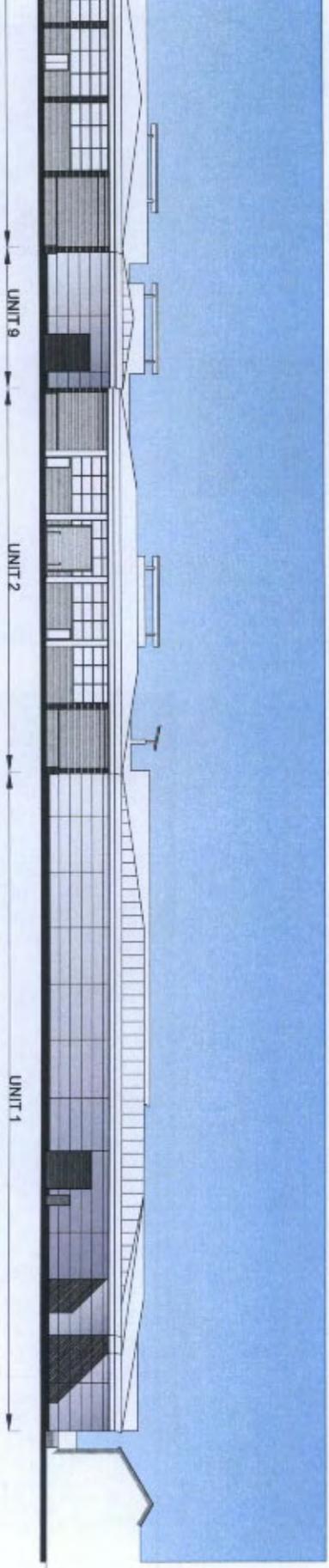
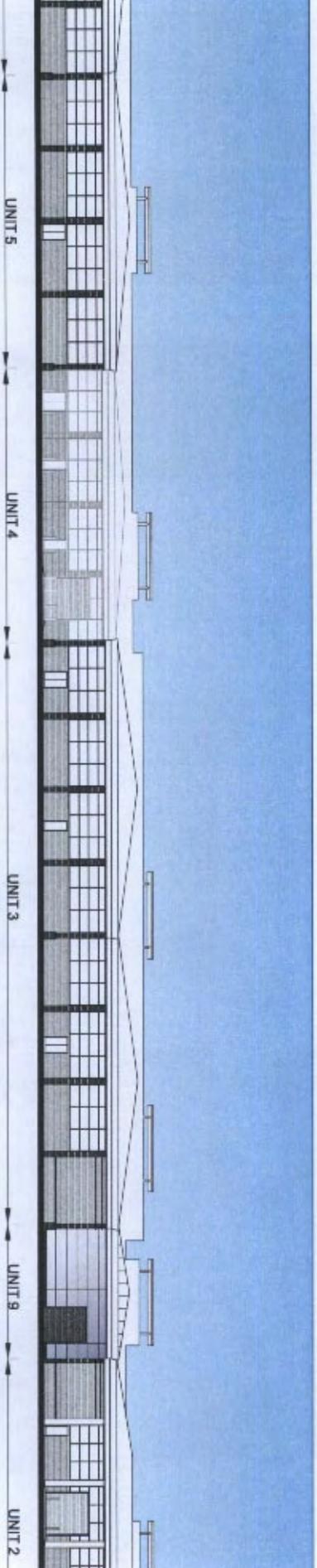
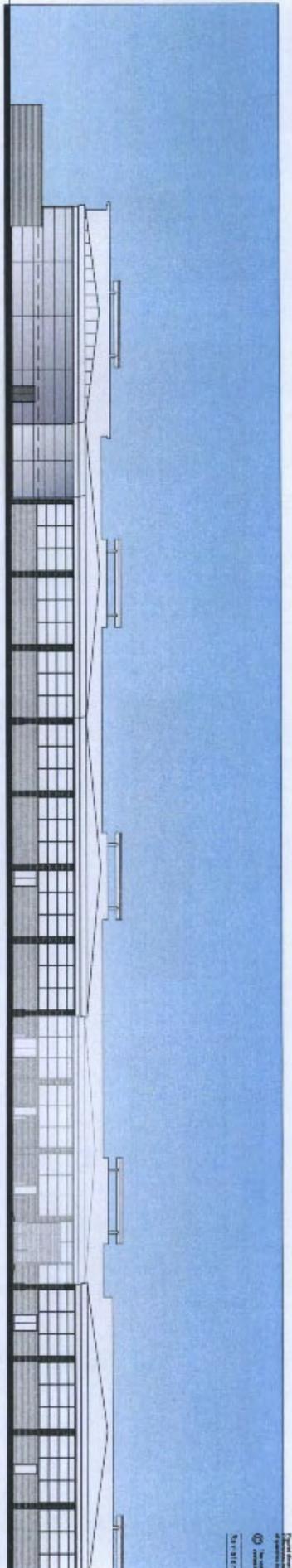
SCALE:
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PROJECT NO.:
 PL 018

DATE:
 11/10/11

PROJECT NO.:
 PL 018

DATE:
 11/10/11



10/1/2018 10:00 AM
 10/1/2018 10:00 AM
 10/1/2018 10:00 AM

CLIENT:
 ROSS AND CUSTODIAN
 TRUSTEES LIMITED
PROJECT:
 87 MIDLANDS GATE
 RETAIL PARK
 CARLISLE
DATE: 12/01/18
SCALE: 1/8" = 1'-0"

DESIGNED BY:
 PROPOSED
 SITE ELEVATIONS
 SHEET 3 FROM 3
 12/01/18

APPROVED BY:
 12/01/18

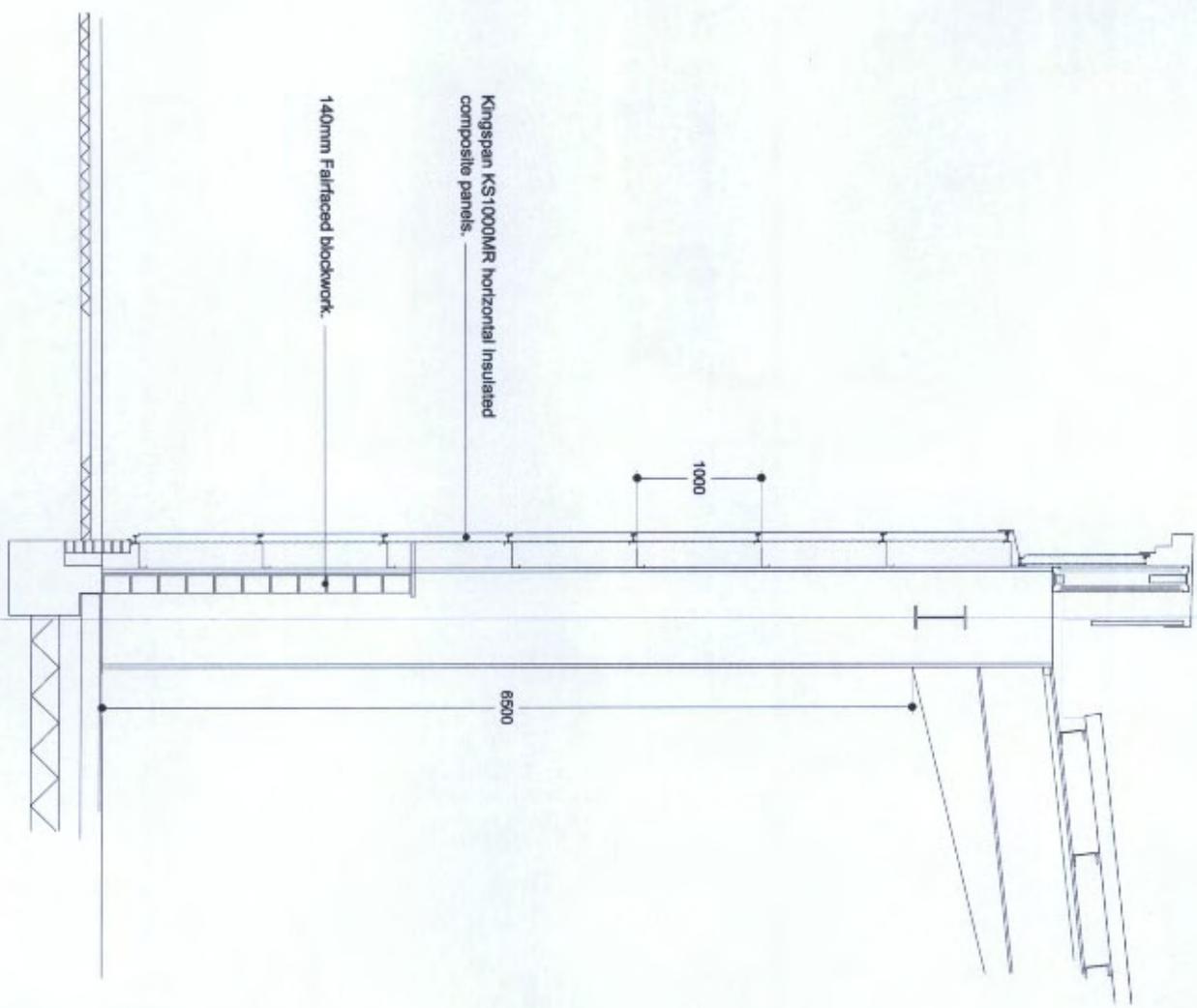
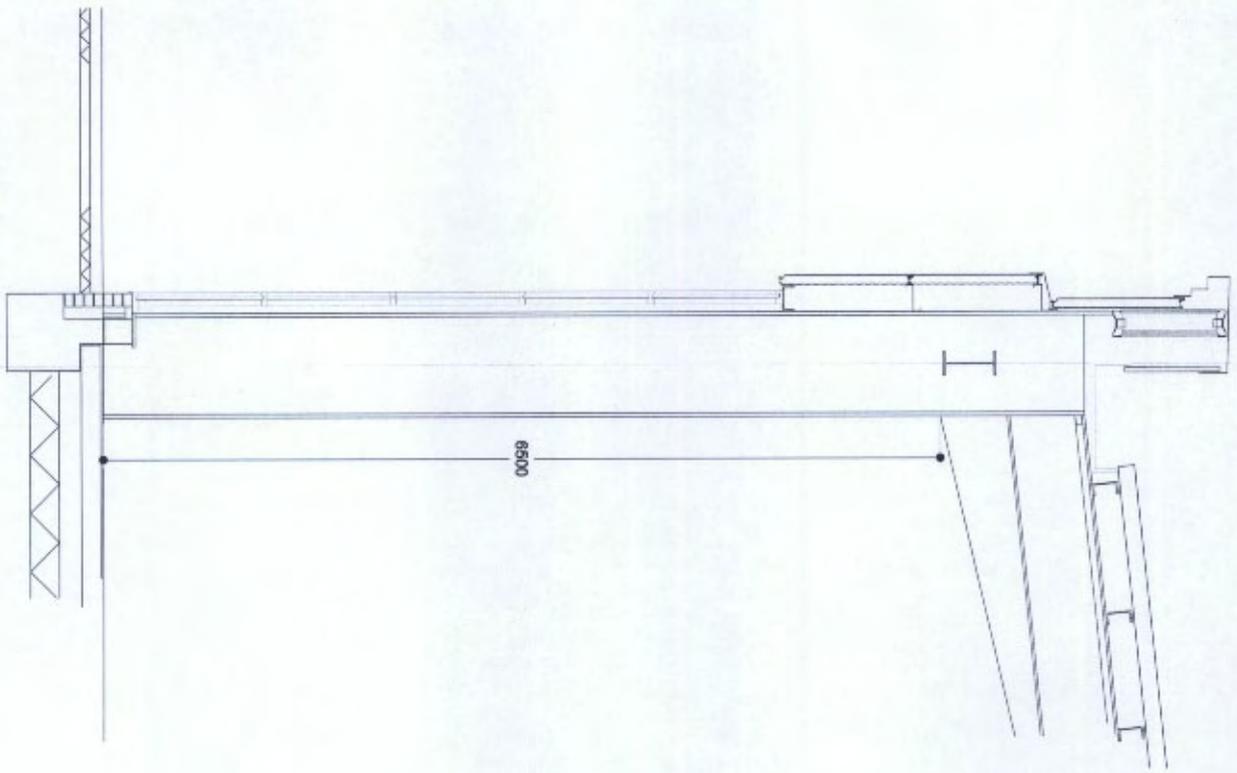
DATE: 12/01/18
SCALE: 1/8" = 1'-0"

PROJECT:
 87 MIDLANDS GATE
 RETAIL PARK
 CARLISLE

DATE: 12/01/18
SCALE: 1/8" = 1'-0"

PROJECT:
 87 MIDLANDS GATE
 RETAIL PARK
 CARLISLE

DATE: 12/01/18
SCALE: 1/8" = 1'-0"



CUSTOMER		PROJECT	
PASSIVUM CUSTOMER TRUSTEES LIMITED		ST. MICHAS GATE BENTAL PARK CARLEIS	
DATE		SCALE	
120		HD	
NO. OF SHEETS		TOTAL NO. OF SHEETS	
788		PL 001	
DATE		BY	
12/01/2011		[Signature]	
REVISIONS		APPROVED	
1. 12/01/2011		[Signature]	



ST. NICHOLAS GATE RETAIL PARK CARLISLE

Design & Access Statement
November 2009

CONTENTS

The Project Team engaged to consider the development potential includes:

- | | |
|---------------------------------|---------------------------------|
| Possfund Custodian Trustees Ltd | Development Managers |
| PRC | Architects and Urban Designers |
| Sovills | Planning & Property Consultants |
| Rowney Shorron | Project Management |
| ACIES | Structural Engineers |

Introduction	2
Assessment	3
• Site Location & Context	
• Planning Design Policy	
Evaluation	6
Existing Site Analysis	
• Land Use	
• Pedestrian Links & Road Hierarchy	
• Legibility, Frontages & Scale	
• Landscape	
• Site Influences - Constraints & Opportunities	
Design Development	12
• Design Principles	
• Land Use & Amount	
• Pedestrian Access & Site Entrances	
• Legibility, Frontages & Scale	
• Landscape	
• Appearance & Materials	
Sustainability	17
Proposals	19
• Layouts	
• Elevations	
Conclusions	21

INTRODUCTION

This Design and Access Statement has been prepared on behalf of Passfund Custodian Trustees Limited, acting on behalf of the owners (Royal Mail Persian Plan) of the St Nicholas Gate Retail Park (SNGRP) at London Road, Carlisle. It forms part of the submission in support of the Reserved Matters relating to planning application 03/1362 for the redevelopment of the site. The redevelopment consent was for:

‘Alterations to existing retail units, part demolition and erection of new retailing warehousing for the retail sale of bulky goods’

The Reserved Matters are for details of siting, design, external appearance of the buildings. The landscaping of the site is concept only the details of which would be conditioned.

The siting and layout was submitted and approved with the application and discussions during 2009 with Council Offices confirm broad acceptance of the layout.

This document will describe the site location and context, acknowledge the national and local planning design policies and guidance, evaluate the site’s opportunities and constraints in light of the preceding assessment, present the proposed development, and explain the design and access solution in detail. Finally, a short conclusion will summarise why the proposal represents the best design for the site given the site characteristics and planning policy context and therefore why the application should be approved.

This Statement has been prepared in accordance with the Commission for Architecture and Built Environment (CABE) guide, ‘Design and access statements: How to write, read and use them’ (2005).



Aerial map, taken from Google, showing the existing site location outlined in red.

ASSESSMENT

SITE LOCATION & CONTEXT

The site is located less than 1 km to the south of Carlisle City Centre. The site is accessed from the A6 London Road and a joint service yard and customer access to the rear from St Nicholas Bridges. The site is broadly triangular in shape and comprises 4.0 ha. The existing site comprises 8 retail units including Halfords, Carpet Right, Netto, a Burger King Drive-Thru and a number of vacant units.

The southern boundary of the site comprises mainly the service yard area adjacent to the historic Settle to Carlisle railway line beyond the site boundary line. The site is on the level with the rail line in a cutting to the south. The south western and western edge of the site is bounded by the St Nicholas Bridges with the road rising up over the rail lines.

To the north, west of the site lies Woodruffe Terrace (a statutorily listed residential terrace of 2 storey properties) separated from the site by a 2 metre high brick wall. The northern edge of the site forms the main front boundary of the site with clear views into the retail park. On the opposite side of the A6 London Road lie the terraced residential properties of Orchard Street, Brook Street, Alexander Street and Oswald Street. The eastern corner of the site (the narrowest point of the site) is bounded by the vacant Railway Inn. The site is bounded by The Botchergate and Corlisle-Settle Railway Conservation Areas to the south, west and east.

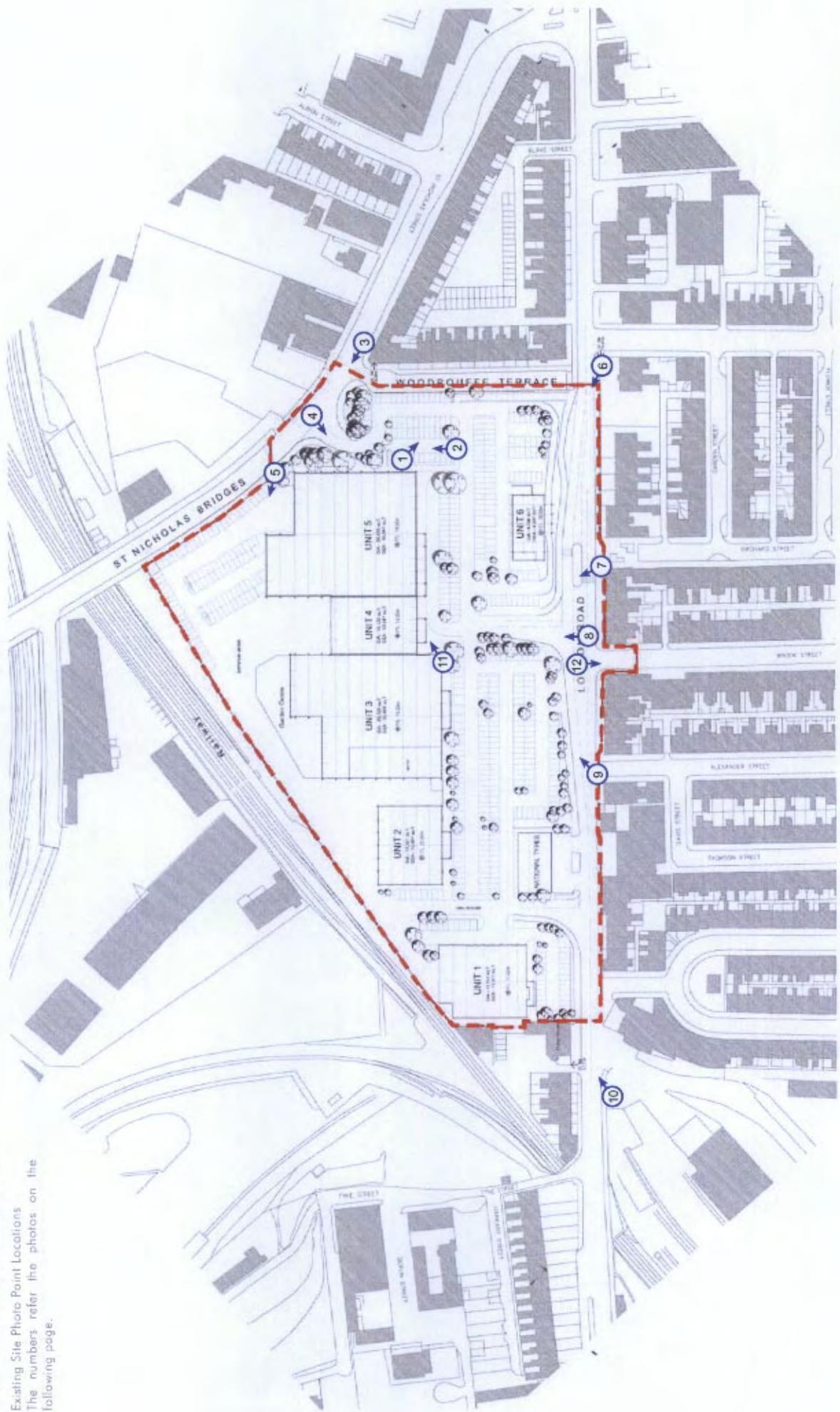


Diagram showing the existing site location
drawings indicative only and not to scale

ASSESSMENT

SITE LOCATION & CONTEXT

Existing Site Photo Point Locations
The numbers refer to the photos on the following page.



ASSESSMENT

SITE LOCATION & CONTEXT



Photo 1 View across existing parking areas towards perimeter wall and Woodruffe Terrace



Photo 5 Existing service yard area



Photo 9 View of north toward existing site entrance



Photo 2 View of existing site entrance from St. Nicholas Brides



Photo 6 View towards the main existing site entrance from north



Photo 10 View north along London Road towards the site



Photo 3 View towards St. Nicholas Brides



Photo 7 Main existing vehicle entrance to the site



Photo 11 Typical existing elevation



Photo 4 View of entrance from St. Nicholas Brides.



Photo 8 View of main existing site entrance



Photo 12 Typical residential scene opposite the site on London Road

ASSESSMENT

PLANNING DESIGN POLICY

Legislative Background

Section 42(5)(a) of the Planning and Compulsory Purchase Act 2004 requires all applications for planning permission to be accompanied by a statement about the design principles and concepts that have been applied to the development.

The requirement for Design and Access Statements with planning applications came into force on 10 August 2006 and the New Article 4C of the GPDO sets out detailed requirements, supplemented by Circular 01/06.

Relevant design and access policies are summarised below.

Planning Policy Statement 1 (Delivering Sustainable Development)

PPS1 (Delivering Sustainable Development) was published by the ODPM in 2005. It sets out the overarching planning policies on the delivery of sustainable development through the planning system.

Amongst the PPS's objectives is that planning should facilitate and promote sustainable and inclusive patterns of urban and rural development by, amongst other things, ensuring high quality development through good and inclusive design, and the efficient use of resources. (para. 5)

Key principles which should be applied to ensure that decisions taken on planning applications contribute to the delivery of sustainable development include:

1. The promotion of high quality inclusive design in the layout of new developments and individual buildings in terms of function and impact, not just for the short term but over the lifetime of the development; and
 2. Consideration of people's diverse needs, aiming to break down unnecessary barriers and exclusions in a manner that benefits the entire community. (para. 13)
- Good design ensures attractive, usable, durable and adaptable places and is a key element in achieving sustainable development. Good design is indivisible from good planning. (para. 33)

Good design should contribute positively to making places better for people (para. 34) and create well-mixed and integrated developments which avoid segregation, in order to create places which function well over the lifetime of the development. (para. 35)

The Carlisle District Local Plan

The Carlisle District Local Plan 2001-2016 was adopted in September 2008. The most relevant policies are reviewed below.

Policy CP5 relates to design and states:

All new development proposals will be assessed against the following design principles. Proposals should:

1. Respond to the local context and the form of surrounding buildings in relation to height, scale and massing, and by making use of appropriate materials and detailing;
2. Take into consideration any important landscape or topographical features and respect local landscape character;
3. Reinforce local architectural features, where appropriate, promoting and respecting local distinctiveness;
4. Ensure all components of the proposal, such as buildings, car parking, access routes, open space and landscaping, are well related to one another to ensure a well integrated, successful and attractive development;
5. Ensure there is no adverse effect on the residential amenity of existing areas, or adjacent land uses, or result in unacceptable standards for future users and occupiers of the development;
6. Ensure the retention and enhancement of existing trees, shrubs, hedges and other wildlife habitats where possible. Where environmental features are lost as a result of the proposal, appropriate mitigation measures should be put in place and on-site replacement of those features will be sought;
7. Include landscaping schemes (both hard and soft) to assist the integration of new development into existing areas and ensure that development on the edge of settlements is fully integrated into its surroundings;
8. Ensure that the necessary services and infrastructure can be incorporated without causing unacceptable harm to retained features;
9. Ensure that the layout and design incorporates adequate space for waste and recycling bin storage and collection.

Although visual appearance and the architecture of individual buildings are clearly factors in achieving these objectives, securing high quality and inclusive design goes far beyond aesthetic considerations. Good design should:

1. Address the connections between people and places by considering the needs of people to access jobs and key services;
2. Be integrated into the existing urban form and the natural and built environments;
3. Be an integral part of the processes for ensuring successful, safe and inclusive villages, towns and cities;
4. Create an environment where everyone can access and benefit from the full range of opportunities available to members of society, and
5. Consider the direct and indirect impacts on the natural environment.

Paragraph 36 sets out that that key objectives should include ensuring that developments:

1. Are sustainable, durable and adaptable (including taking account of natural hazards such as flooding) and make efficient and prudent use of resources;
2. Optimise the potential of the site to accommodate development, create and sustain an appropriate mix of uses (including incorporation of green and other public spaces as part of developments) and support local facilities and transport networks;
3. Respond to their local context and create or reinforce local distinctiveness;
4. Create safe and accessible environments where crime and disorder or fear of crime does not undermine quality of life or community cohesion;
5. Address the needs of all in society and are accessible, usable and easy to understand by them; and
6. Are visually attractive as a result of good architecture and appropriate landscaping.

Paragraph 38 sets out that design policies should avoid unnecessary prescription or detail and should concentrate on guiding the overall scale, density, massing, height, landscape, layout and access of new development in relation to neighbouring buildings and the local area more generally. Local planning authorities should not attempt to impose architectural styles or particular tastes and they should not stifle innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles.

ASSESSMENT

PLANNING DESIGN POLICY

Policy CP9 relates to **Development, Energy Conservation and Efficiency** and states:

Development proposals should take into account the need for energy conservation and efficiency in their design, layout and choice of materials. The principles should be introduced in the early stages of the design process in order to consider the orientation of buildings to maximise solar gain coupled with high levels of insulation to reduce heating costs. The efficient and effective use of land, including the reuse of existing buildings and the use of environmentally sustainable materials should also be encouraged. Landscaping schemes also may be used to shelter buildings in exposed positions to reduce heat loss.

These elements will contribute to the energy efficiency of a new development.

Developers should also consider the possible incorporation of photovoltaic cells, active solar panels and other small-scale sources of renewable energy. Consideration should be given to recycled materials, waste minimisation and recycling measures within the design.

Designers will be encouraged to include systems for collecting roof water to enable its re-use.

POLICY CP15 relates to **Access, Mobility and Inclusion**. It states that:

Development proposals should make provision for easy, safe and inclusive access to, into and within buildings and facilities. The layout and design of developments should meet the requirements of accessibility and inclusion for all potential users regardless of disability, age or gender. The Council will have regard to the following criteria when assessing development proposals:

- 1. The design of entrances and exits and ease of permeation through and between developments in terms of street furniture, circulation areas and pedestrian routes;*
- 2. The location of any development proposal in relation to its potential users;*
- 3. Accessibility to all transport modes and provision of adequate parking for disabled people;*
- 4. Provision of on-site facilities such as public toilets and appropriate signage*

POLICY CP17 relates to **Planning Out Crime**. It states that *'the design of all new development must contribute to creating a safe and secure environment, integrating measures for security and crime prevention and minimising the opportunity for crime.*

The following points should be applied to all development proposals:

- 1. Security measures should be an integral part of the design*
- 2. Developments should be laid out and buildings positioned to maximise natural surveillance with the intention of creating a sense of neighbourhood and deterring criminal and anti-social activity*
- 3. Public and private spaces should have clearly defined boundaries.*
- 4. Footpaths and cycleways should be designed to maximise their use and prevent opportunities for concealment, unauthorised access or provide a choice of escape routes.*
- 5. Landscaping schemes be designed to ensure that they do not create secluded areas, opportunities for climbing or reduce natural surveillance.*
- 6. Lighting should deter criminal and antisocial activity whilst minimising light pollution. CCTV may be considered necessary in certain circumstances. Developers should, at the earliest stage possible, consult the Architectural Liaison Officer to advise on measures to be incorporated for designing out crime.'*

EVALUATION

EXISTING SITE ANALYSIS

Land Use

The site comprises the 8 retail units (including a sub-let unit) with ancillary service yards, parking, landscaping and pedestrian walkways. The site is surrounded by the railway land of the Settle to Carlisle historic railway, commercial units to the west, south and east, and residential properties, including the listed Woodruffe Terrace, to the west and north and bounded by The Botchergate and Carlisle-Settle Conservation Areas on 3 sides.

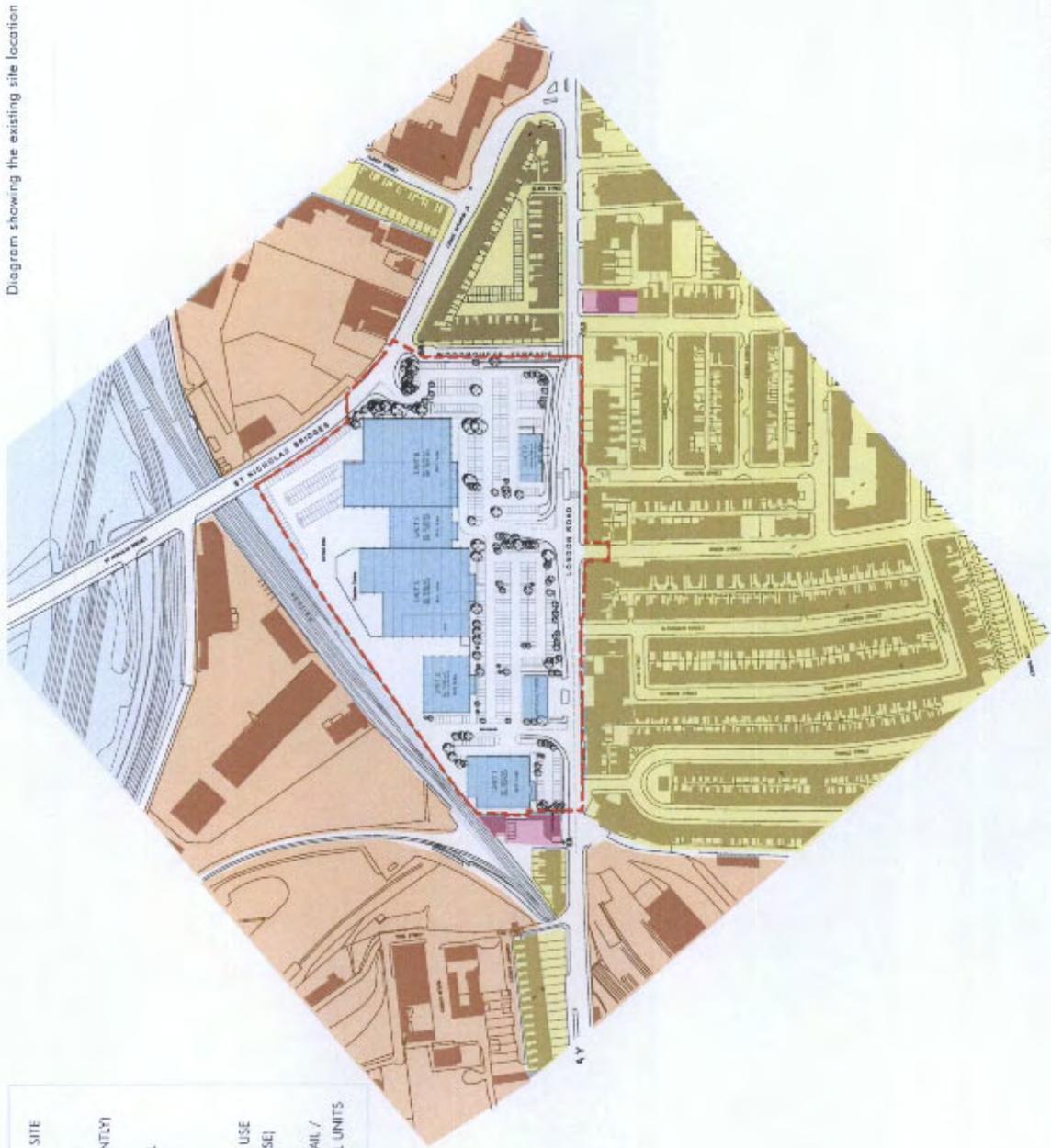


Diagram showing the existing site location

Drawings indicative only and not to scale

EVALUATION

EXISTING SITE ANALYSIS

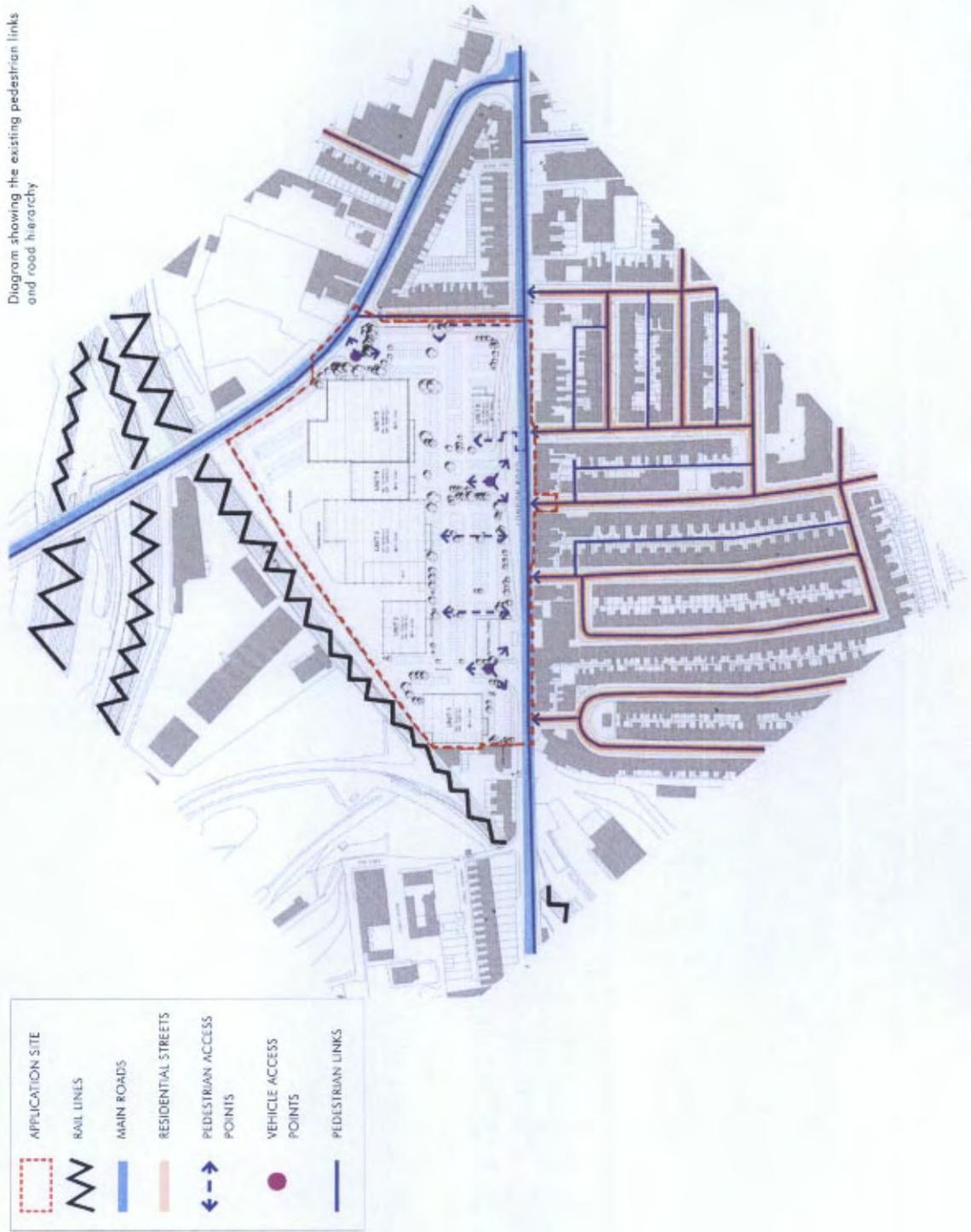
Pedestrian Links & Road Hierarchy

Pedestrian access is restricted to the A6 London Road frontage and an access point to St Nicholas Bridges to the south western corner of the site. The remainder of the site is bounded by security fencing on to the rail lines, a brick wall onto Woodruffe Terrace and the buildings facing the Railway Inn.

There are 6 pedestrian access points from the A6 London Road frontage 2 of which are shared with the vehicle access points from this road. These pedestrian access points provide variety and choice of routes but there is serious pedestrian/ vehicle conflict within the site resulting from poor delineation of parking and pedestrian areas.

There are excellent links to the Carlisle centre with a main bus route and bus stops immediately adjacent to the site and the A6 London Road.

The current layout of the site also encourages a 'rat-run' through the site for vehicles between the A6 London Road and the St Nicholas Bridges access point at the rear of the site. This adds to the pedestrian vehicle conflict and significant security issues. Conflict also exists with service vehicles from St Nicholas Bridges and customer vehicles.



drawings indicative only and not to scale

EVALUATION

EXISTING SITE ANALYSIS

Frontages & Scale

The SNGRP is a traditional style retail park with the access frontage to the units predominantly set back from the main A66 London Road street frontage. One exception to this is the Burger King Drive-Thru.

The former tyre centre was brought into the overall site in the early 2000's having previously been a stand alone unit. This unit presents a poor and incongruous frontage to the main road. The Burger King has a side frontage facing the entrance into the site.

Existing building heights vary between 8-10 metres for the main retail units which is typical of the surrounding commercial uses. The surrounding residential buildings comprise mainly 2 storey properties although those on London Road tend to be larger in scale.

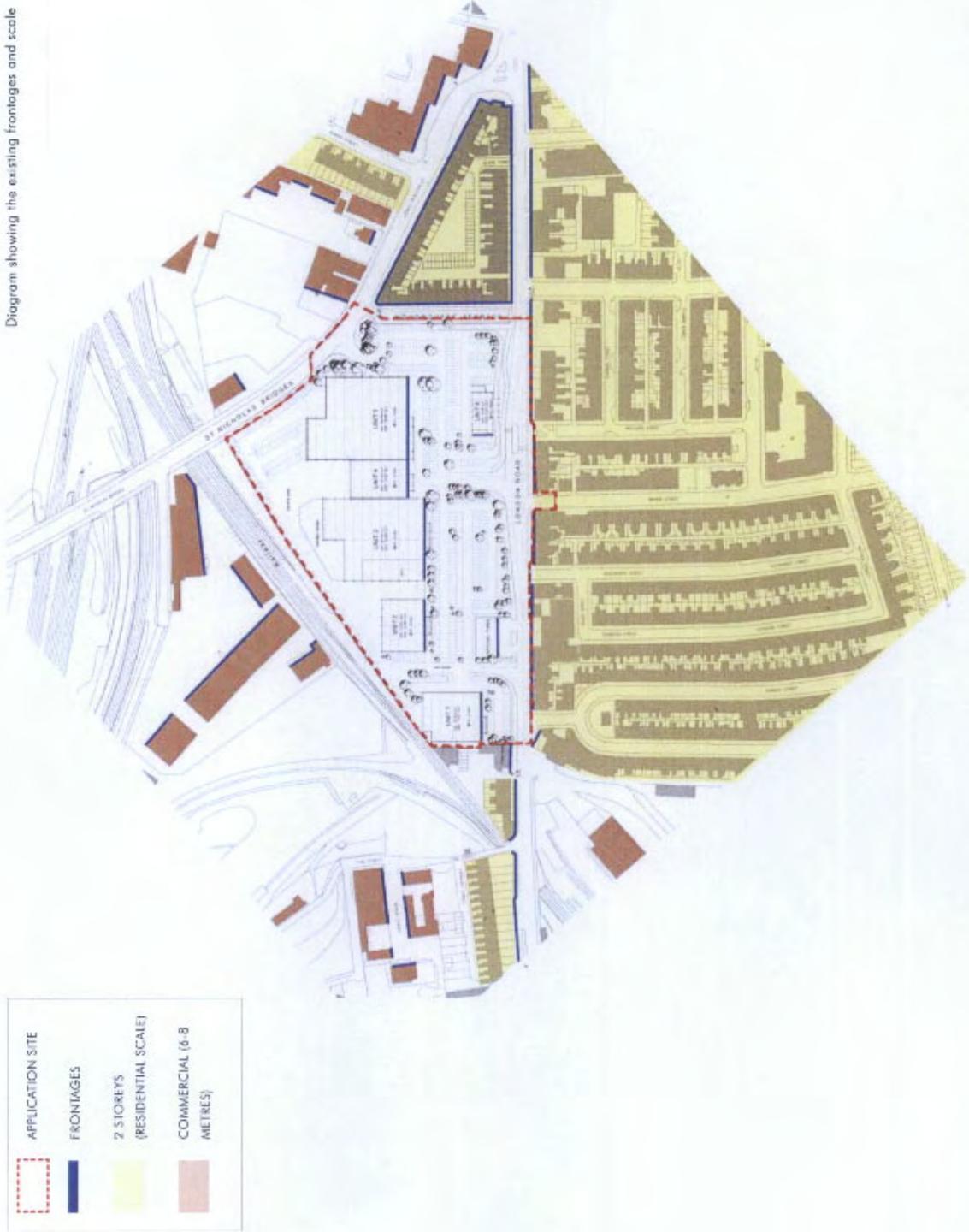


Diagram showing the existing frontages and scale

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EVALUATION

EXISTING SITE ANALYSIS

Landscape

There is limited & poor quality soft landscape within the site boundary with a few trees and some green shrubs and grassed areas at the entrances. There is a tree band adjacent to the length of the southern boundary. The hard landscape comprises tired parking surface treatment and non-descript customer walkway surfaces. A 2 metre high red brick wall exists on the west boundary with Woodruffe Terrace and a low 0.75 metre high wall along part of the frontage with the A6 London Road.



Diagram showing the existing hard and soft landscape areas on the site.



drawings indicative only and not to scale

EVALUATION

SITE INFLUENCES

Constraints

- The boundary constraints comprise of:
- a poor quality security fence leading onto the Sella to Carlisle railway to the south
 - a 2 metre high brick wall to the west separating the site from the listed Woodruffe Terrace together with the rising road known as St Nicholas Bridges
 - the busy A6 London Road to the north
 - the listed Railway Inn to the east, and
 - Conservation Area status on 3 sides (east, south and west).

The site itself has a number of constraints including:

- No street presence or relationship with the residential properties on the opposite side of the A6 London Road.
- Confusing frontage with 2 vehicle access points, 6 pedestrian access points and low quality shrubs.
- A 'rat-run' route through the site for cars with a total of 3 vehicle access points (2 from the A6 London Road and 1 from the rear access to St Nicholas Bridges).
- Tired and run down pedestrian and vehicle environment with poor quality hard surface areas and natural environment.

A key constraint of the site is security. There is no security gate to the service yard area to the rear of the site. The site is open to the rear, which has resulted in the vocalisation to the rear fence along the southern boundary and potential for trespassers on the rail lines. Furthermore, the possibility of 3 'escape' routes from the site increases the security constraint issue.

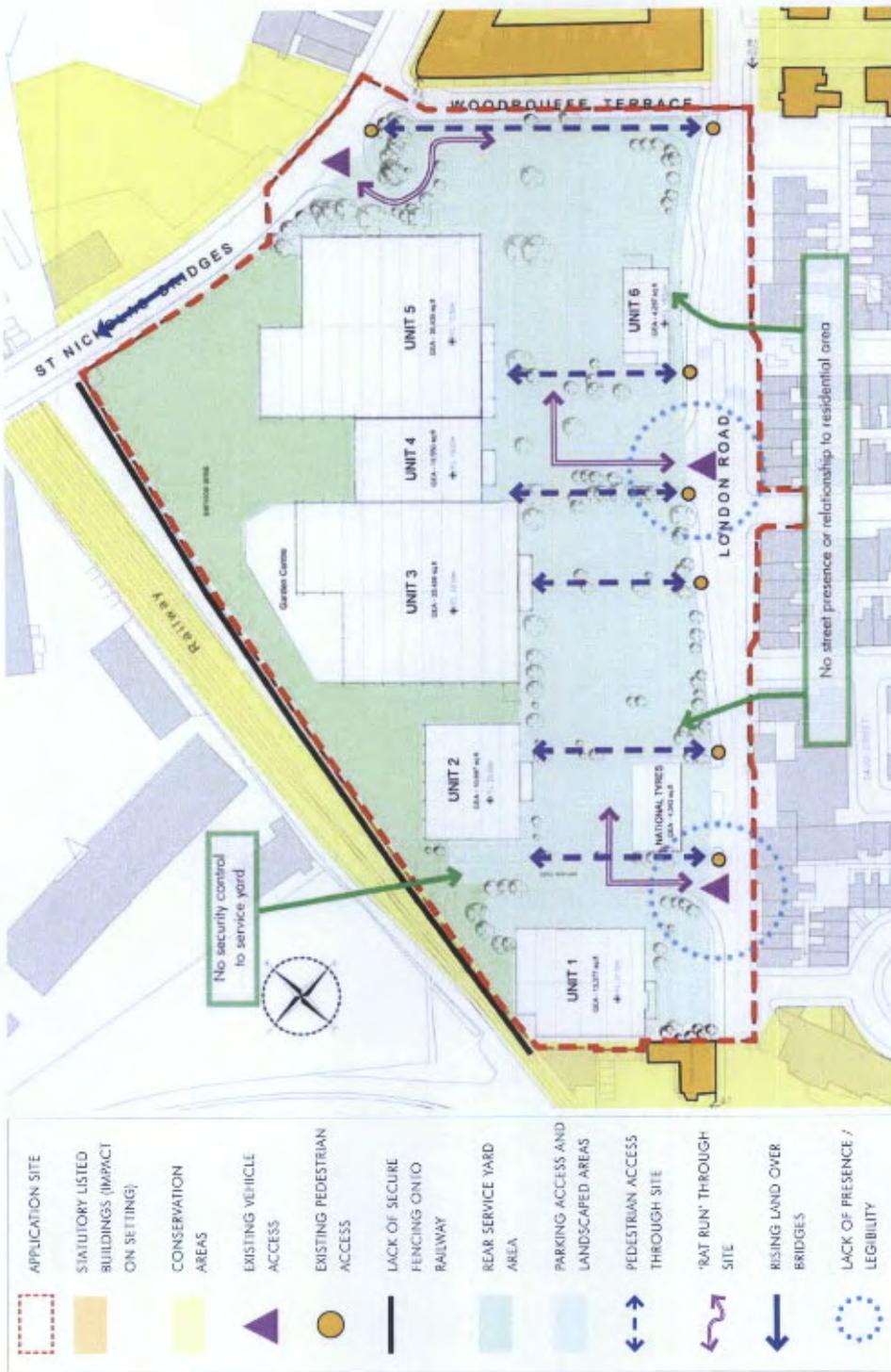


Diagram showing the opportunities of the existing site

drawings indicative only and not to scale

EVALUATION

SITE INFLUENCES

Opportunities

Significant opportunities exist to further uplift the site and enhance the setting of the Listed Buildings and Conversation Areas. The approved consent will go some way to achieving this with the revised access points, and the submitted Reserved Matters will further achieve these aims.

Opportunity exists to improve site security with the introduction of additional units to enclose the built form and create a secure length of 'street' frontage, and/or through the introduction of security gates, together with the replacement of the security fencing and the removal of the secondary site access to vehicles from St Nicholas Bridges. The applicant has entered into a Section 106 agreement to operate a CCTV system which will link into the wider existing CCTV network in the surrounding area.

The introduction of a single customer vehicle access point as approved will enhance the site presence along the A6 London Road. This would also allow for an improved junction access. This has been approved by the planning consent for the site.

The site presence on the A6 London Road frontage could be further enhanced through the introduction of series of new signs of prominent locations visible from the approaches to the site.

The introduction of an additional unit on the opposite side of a new entrance to Burger King and more centrally located on the frontage, would provide a 'gateway' to the site and provide a sense of arrival on the site.

Opportunity exists to enhance the historic fabric of the listed terrace of properties known as Woodruffe Terrace

There is potential to rationalise the existing car parking and pedestrian access within the site with additional sensitive landscaping making it more user friendly.

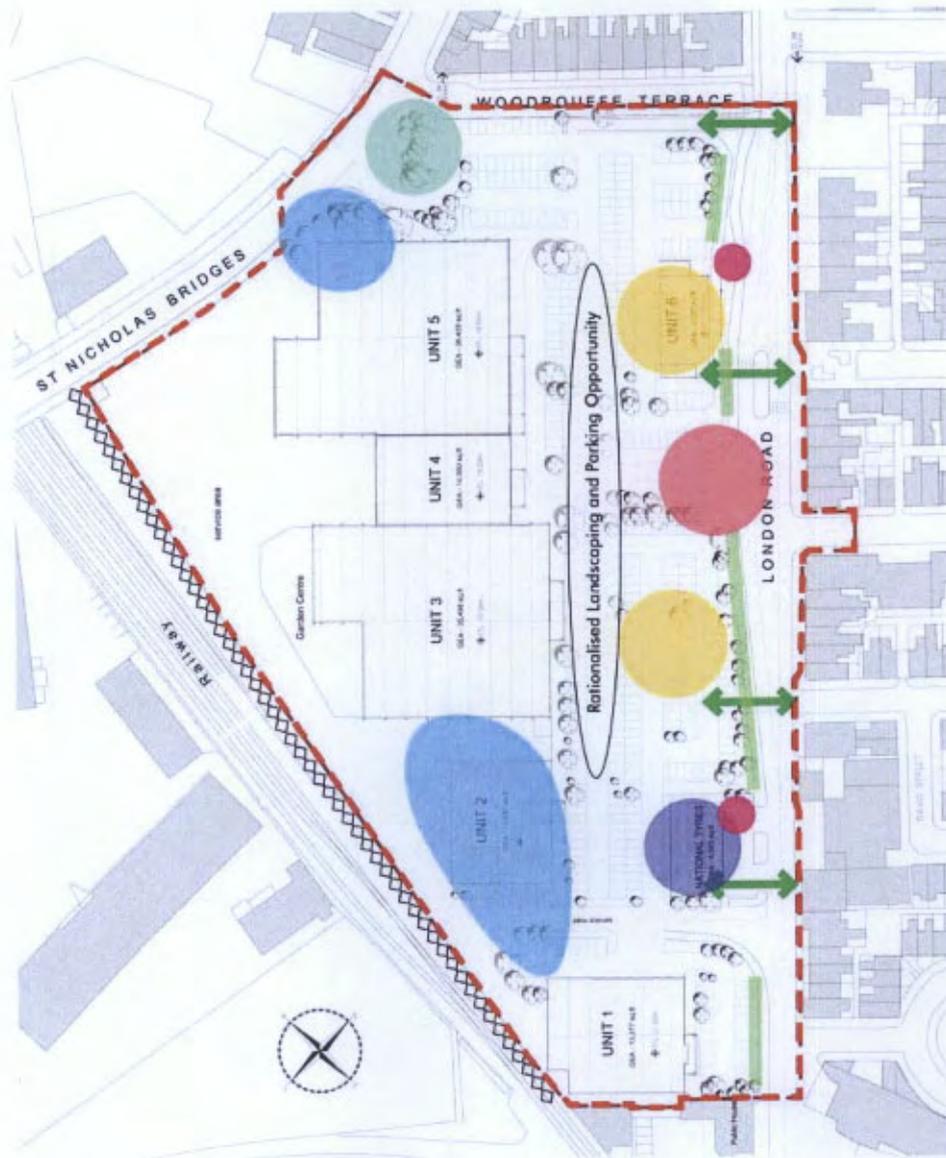


Diagram showing the opportunities of the existing site

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DESIGN DEVELOPMENT

DESIGN PRINCIPLES

The principle of the development and access have been agreed at outline stage. The design details of the site have considered design principles related to the improvement of a retail park.

The evaluation of the current site, its location, and the design development of the scheme has highlighted seven guiding principles against which the design of the scheme has been considered:

- i. A greater sense of street presence should be created along the main frontage to ensure the retail park links with its surroundings and context. This could be achieved through realigned road systems, additional 'gateway' buildings, signage, and improved landscaping.
- ii. A more cohesive approach to the siting of the buildings should be pursued to ensure a useable and legible site.
- iii. The parking areas should be consolidated and located close to the buildings they serve in order to create a more formal and legible space.
- iv. Every opportunity should be taken to create a greater choice of routes for pedestrians to increase permeability.
- v. A variety of unit sizes should be provided to add to the degree of choice available to retailers and their customers to increase the vitality of the location and enhance the destination of the centre.
- vi. An improved and maintained landscaping scheme should be achieved to soften the appearance of the site, whilst recognising the need to retain the views of the retail units for pedestrians and vehicles users.

LAND USE & AMOUNT

The use of the application site remains the same as currently (retail and drive through restaurant) within an increase in the number of building units.

The current site consists of 8 units totalling 115,191 sqft.

The proposals will include the demolition of 2 units and part of the rear of Unit 6, which will total 19,169 sqft.

4 new units will then be erected totalling 48,653 sqft.

This will provide a total gross external site area of 144,675 sqft.

DESIGN DEVELOPMENT

PEDESTRIAN ACCESS & SITE ENTRANCES

The removal of 2 of the 3 vehicle entrances will significantly enhance the sites presence by focusing the entrance to the site in a central location close to the existing pedestrian crossing. The associated junction improvements will further add to the legibility and presence of the site.

The use of the rear access for service yard use only will remove the 'rat-run' possibility and enhance the security of the site. It will also allow for the rationalised parking areas that will aid the customers of the site.

Pedestrian access of the site is further improved through the rationalising of the entrances and the parking spaces. The clear delineation of the spaces and use of different materials for the parking spaces and the pedestrian access routes will aid the navigation and ease of movement through the site for pedestrians.

Overall the improvements will substantially enhance pedestrian safety.

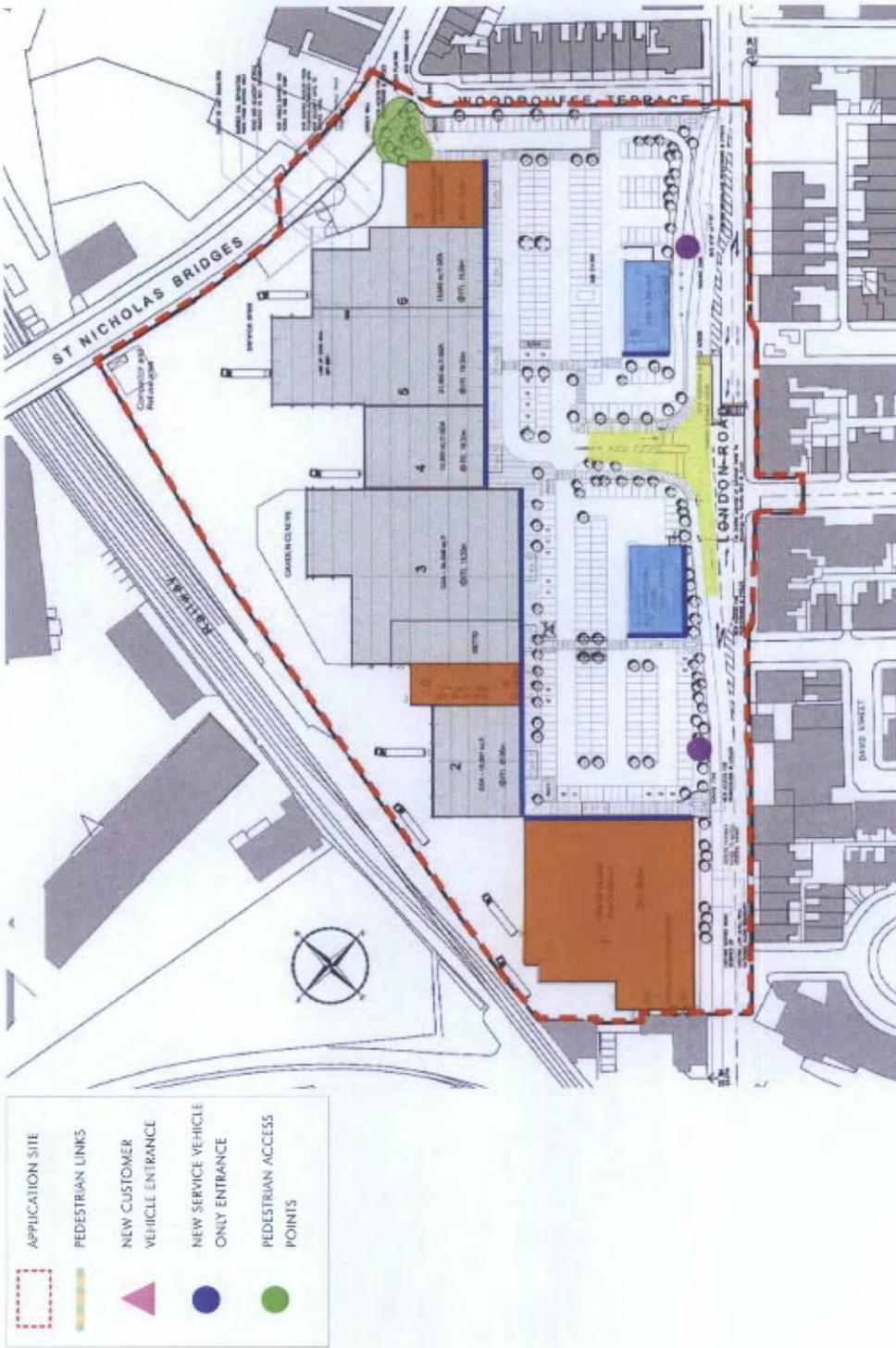


Diagram showing the proposed pedestrian access and site entrances

drawings indicative only and not to scale

DESIGN DEVELOPMENT

LEGIBILITY, FRONTAGES & SCALE

The active frontages of the proposal will predominantly face the A6 London Road. The new Unit 1 will face the rationalised parking areas and will help enclose the space creating a legible and cohesive retail park with a continuous length of 'street' frontage. The introduction of the new Unit 10 will create a 'gateway' to the site offsetting the Berger King unit on the opposite side of the entrance.

The scale of the proposed units will be similar to the existing units being between 8-10 metres in height. Unit 1 will be 9.3 metres in height. Due to the level changes sloping down when approaching from the south east, the impact will be to give the impression of a lower building.

When viewed from the A6 London Road the building will be 6.3 metres high adjacent to the Railway Inn. The mass of this facade is broken by the change in materials and change in roof design whereby the central element of the unit rises to 7.46 metres, before reaching its full height towards the north west of 9.3 metres. The overall massing is further reduced by the stepping down of the boundary wall towards the north west. The effect is to reduce the impact on the adjacent listed Railway Inn.

The height of the remaining units differs due to the variety of roof forms proposed. The existing units on the site have various pitched designs which will be hidden behind parapets when viewed from the street level. The proposed units and the refurbished facades of the existing units present a bright contemporary approach to bring new life to the retail park. This crisp, cool aesthetic will act as a back drop to the animated shop frontage.

The varying heights of the building, roof designs and use of materials have been proposed to break down the overall scale and massing of the units and to provide a more visually pleasing architectural form to uplift the local environment and encourage greater customer use.

The proposed new units and refurbished existing unit frontages will provide a comprehensive, attractive and contemporary outlook, whilst the landscaping and rationalised pedestrian / parking areas will provide the cohesion to the site. This will ensure the proposed development sits comfortably with the existing area providing a much needed uplift to the site.

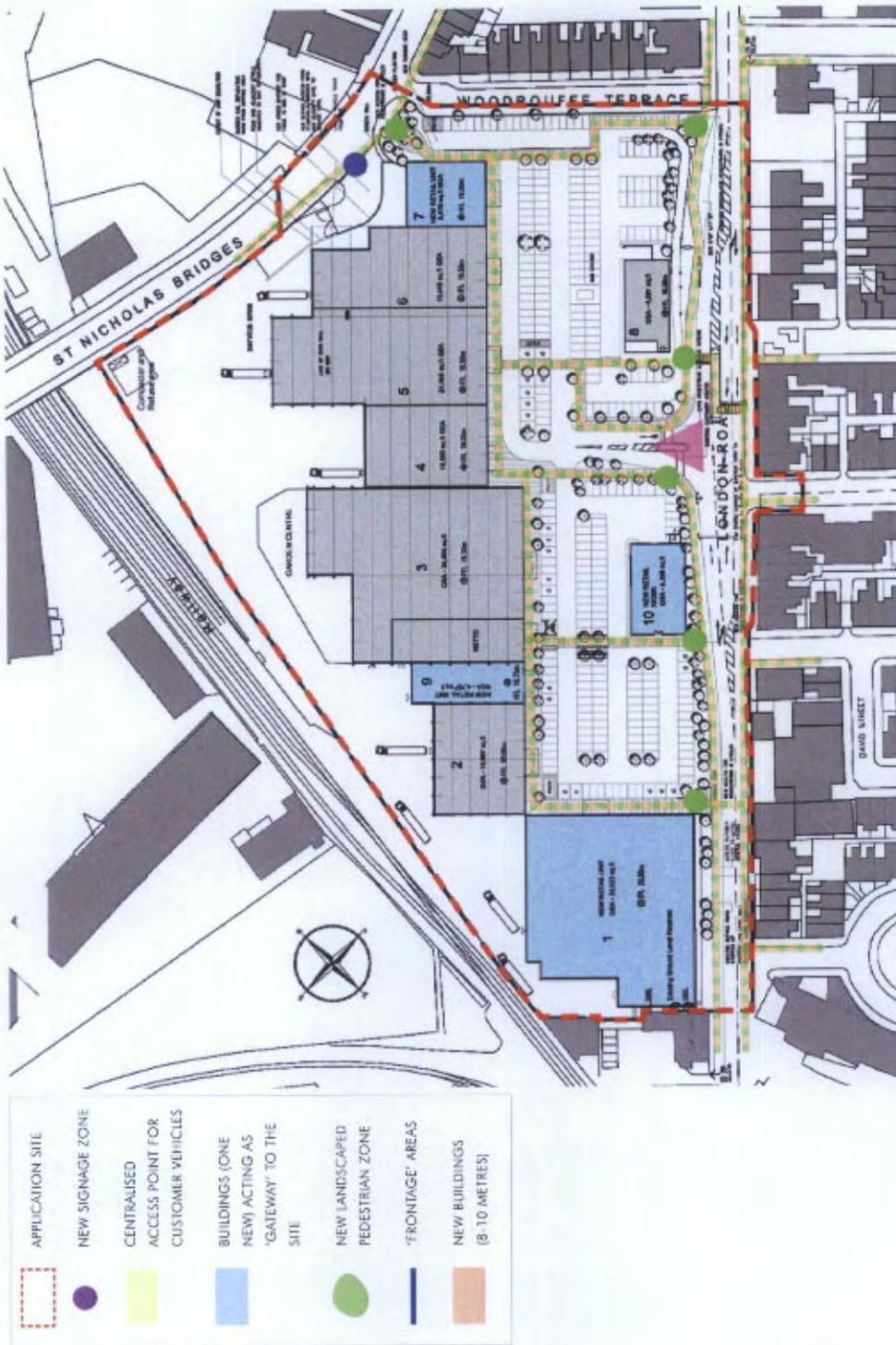


Diagram showing the proposed frontages, legibility and scale

drawings indicative only and not to scale

DESIGN DEVELOPMENT

LANDSCAPE

The nature of a retail park such as SNCRRP is that the units have to be visible from the street frontage in order to attract business and passing trade, whilst forming a balance with the local micro site characteristics. Therefore the type, number and location of trees requires careful consideration.

A large proportion of the existing trees across the site are retained and utilised to lead pedestrians. Unit 1 is set back from the boundary with the A6 London Road by 5.5 metres and 2 groups of existing trees are retained along this frontage. A further 2 semi-mature trees of 5 metres high are proposed at the boundary of the site with the Railway Inn. These trees, together with the hedge planting will help soften the appearance of this facade and improve the setting of the Railway Inn.

A further 2 groups of semi-mature trees of 4 and 5 metres high is proposed between Unit 1 and Unit 10 on the A6 London Road boundary. The positioning of these trees has been designed to lead the pedestrian to one of the pedestrian entrances along this frontage and to provide a landscape setting for Unit 10.

A series of 10 trees of 4 m high trees is proposed adjacent to the pedestrian footway at the edge of the new customer entrance to the site. This again helps lead customers into the site and provides a landscaped environment for key pedestrian route in the site. This is further emphasised by 2 substantial trees of 7 metres high being located at the end of this footway.

A group of 7 semi-mature trees of 3 and 4 metres high are proposed at the corner of the site where it meets Woodruffe Terrace which will partially screen the back end of the Burger King

Drive-Thru.

The proposed service entrance to the rear of the site, and the new pedestrian access only point onto St Nicholas Bridges will have a revamped landscaping scheme including 9 new semi-mature trees and screen wall which will highlight this access point.

There will be a dispersion of trees at intervals around the site at the end of parking bays and along the walk ways. These are limited due to the requirement to ensure visibility of the units.

The pedestrian walkways will be treated with a different hard surface material to the parking bay surfaces. This will aid delineation of the main customer areas outside the units; it will increase ease of movement and legibility through the site, and will uplift the local environment.

The key element of the landscape concept is to uplift the local environment which is currently run down, in a way that will ease the movement for pedestrians and create a safer environment for the pedestrian on the site, whilst recognising the need that retail parks attract a large number of car movements. The landscape also takes account of the need to ensure the units themselves are clearly visible from the main road, and the entrances visible to aid legibility and orientation within the site.

The details of the landscape scheme will be conditioned.



Diagram showing the proposed landscape

DESIGN DEVELOPMENT

APPEARANCE & MATERIALS

The current site frontage has a broken series of units of a very dated style. This application seeks to provide a comprehensive and contemporary 'face lift' for the park, providing an appealing place which will attract both retailers and customers.

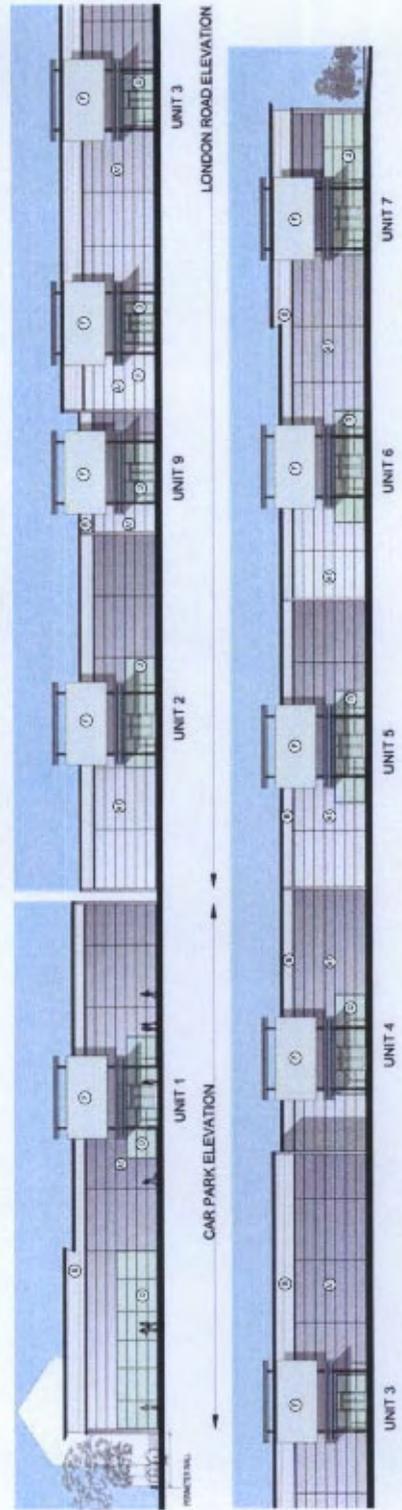
The proposed elevations are simple and crisp and do not seek to copy the style of the adjacent conservation area; but rather present a contemporary design approach, appropriate to the local community it serves.

The proposed scheme creates the opportunity to significantly uplift the existing appearance of the site through the introduction of a more contemporary form and finishes. The use of a new palette of materials will include composite cladding panels (of a limited palette of colour), flat metal and profiled built-up cladding, and curtain wall glazing. The colours and materials proposed will brighten up the immediate surroundings and promote a contemporary aesthetic.

One key improvement will be the introduction of signing zones at the main road frontage, and new signage features above each of the unit entrances. The introduction of these entrance features, increased parapets hiding the shallow pitched roofs, together with a greater sense of continuous frontages will aid the navigation and orientation around the site.

MATERIALS KEY TO ELEVATIONS

- (A1) Composite microline cladding panels
- (A2) Composite microline cladding panels (contrasting colour)
- (B) Profile metal cladding
- (C) Flat metal cladding
- (D) Curtain wall glazing
- (E) Profile metal built-up cladding
- (F) New signage feature



SUSTAINABILITY

STATEMENT

Sustainability is one of the core principles behind the proposed buildings of SNGRP and will feature in what form of construction should take place and has influenced the choice of materials to be used. This section emphasises the design, construction and access issues associated with the sustainability of the proposal.

Rationale

The intention is to make the best use of this brownfield site by optimising the development opportunity with additional units whilst refurbishing the existing units to create an attractive, comprehensive and sustainable redevelopment.

Design

The scheme has been designed to address microclimate issues associated with the existing environment. Buildings and associated structures are located to avoid any wind tunnel effect and to prevent solar glare, although solar gain will be promoted through energy saving benefits to be considered.

Energy Efficiency

The applicant intends to reduce the carbon footprint of the development in line with national policy. The applicant will consider the use of improved building fabric, improved Air leakage and the use of Low Carbon Technologies in order to achieve this reduction. The approach to delivering this requirement will consider the following:-

- Analysis of the deliverables total predicted Energy demand and Carbon Footprint for the new and existing units and seek to improve on this.
- Analysis of each option to demonstrate:-
- What heat, cooling or power demand the technologies would displace
- What contribution each would make towards the carbon reduction target
- Key issues, risks and practical implications of each selection.

The proposal will ensure that the building does not suffer any unacceptable heat loss or gain. The building specification will include such measures as thermally broken sealed double-glazed windows and doors, insulated cladding to walls, roof and loading doors.

The selection of heating system will take into consideration the aim to minimise emissions of carbon dioxide.

Low energy lighting will be fitted in accordance with Approved Document L2A, utilising low energy lamps and energy saving controls such as daylight sensors and occupancy detectors.

Insulation standards will, as a minimum, be in accordance with Approved Document L2A.

The installation of external light fittings, which are designed for energy efficiency and adequately controlled, will be addressed at the detail design stage.

Energy Strategy/ Renewables

A comprehensive Energy Strategy will be considered identifying the initiatives being considered and ultimately incorporated.

- Buildings will be designed to maximize thermal performance and minimise air leakage. (Thermal models will be provided as appropriate).
- LED feature lighting will be considered.
- Wherever practical the use of Green Electricity will be considered for all landlords and tenants supplies.
- All tenants will be encouraged to incorporate Heat Pump systems (Low Carbon Technologies)
- Gas condensing boilers will be incorporated into all landlords' installations.
- All landlords' installations will incorporate smart metering.
- All tenants will be encouraged to incorporate smart metering as part of their fit out works.
- The site lighting will incorporate night setback facilities in order to reduce energy consumption, minimise light pollution and disruption to neighbours.
- A proportion of the roof area run off will be collected via a Rainwater Harvesting facility and re-used for irrigation purposes.

Flexible Space

The scheme will contain a range of unit types and sizes to provide the maximum appeal to potential occupiers.

Appreciating this, the majority of the larger units are to be constructed on a modular basis, allowing easy vertical subdivision, whilst much of the space will also facilitate insertion of mezzanine floors. This will increase the attraction of the units to operators, enabling the retail spaces to be easily adjusted to respond to the needs of users, whether these are for expansions, or indeed reduction of floor space.

Water Efficiency

Installation of low flow appliances such as supply of restrictor valves, low flow showerheads, spray taps and dual flush toilets will be standard in landlord areas and encouraged in tenant areas. Although simple solutions, the combination of these methods can bring significant savings.

Public Realm

The buildings were designed to encourage interaction and the use of amenity spaces by prospective staff and visitors alike who will assist in creating a secure and safer environment.

SUSTAINABILITY

Construction

Use of Materials

Where it is not possible to utilize the existing building materials in their current form, they will be broken up and used as base material for the paved and built areas.

The buildings comprise mainly of steel, concrete, timber and glass all of which have long life spans and can be successfully recycled at the end of the building's life. Where practicable possible, materials will be from responsible sources as identified by accredited schemes. Insulation materials will be selected from a range with low global warming potential.

Warming Potential

Timber products will be from an acknowledged certification scheme with chains of custody or equivalent.

Locally sourced materials will be used where feasible.

Paint finishes with low odour and low toxicity will be specified.

Site Waste Management

With respect to waste and waste management the scheme will incorporate materials with long life spans, including steel, concrete and glass all of which are successfully capable of being fully recycled.

The contractor will be encouraged to reuse suitable building waste, such as crushed concrete, for the construction of this development.

The contractor will also be requested to reduce waste and comply with Regulations regarding the disposal of waste, i.e. Hazardous, non-hazardous or inert. The contractor will be required to provide a waste management plan that also addresses recycling of surplus building materials.

The site levels will be set such that minimal material will be either taken away or brought to site to make up levels.

The existing 'topsoil', where suitable, will be reused on the site. The contractor will be required to store topsoil correctly so that it can be effectively reused. This will aid the reduction in carbon emissions from the construction as less traffic will be required, which will also mitigate the site impact on the neighbourhood.

It is anticipated that the recycled and secondary aggregates achieved from all demolition works will be suitable for reuse i.e. for concrete aggregates for paved roads, aggregate sub-base and as a base for building foundations.

The provision of bin stores recycling facilities has been provided in the rear serviced yard area.

Wider Sustainability Issues

Designing Out Crime

Cumbria Police is concerned that schemes such as this improve the security of the site. The proposed scheme significantly enhances the security of the site.

Access and Movement; well-defined routes, spaces and entrances that provide convenient movement have been incorporated. Quality of connections helps to reduce the opportunity to commit crime.

Physical Protection and Activity; by nature a retail park is a busy place during opening hours and human activity creates a reduced risk of crime and a sense of safety. Compatible uses help to prevent potential conflict. Outside of the hours of use the existing site has significant security issues – there is an opportunity for rat-runs for vehicles with no security gates to the rear of the site giving potential to access the railway beyond.

The proposed scheme significantly improves the security issues on the site by removing the access to the rear with the introduction of the new units enclosing the space and a security entrance to the service yard. The replacement of the fencing along the boundary with the railway will also improve security.

Structure; the positions of the buildings within the overall layout relate to one another to aid with the safety and sustainability of the entire site. Unit sizes, entrance locations etc help to create places robust enough to cope with changing requirements giving the flexibility for the site to evolve.

Surveillance; There are no concealed entrances in the scheme. The entrances to the store are clearly visible from the walkways and the car parking areas are also visible from the walkways and road entrance. This natural passive surveillance helps deter criminals. Active surveillance in the form of a full cover CCTV system will be linked into the local authority network.

The contractor will be advised to actively discourage theft and vandalism during the construction period and the site will remain 'restricted access' during the construction works to discourage casual crime and assist in maintaining a safe site from a health and safety perspective.

The developer and their professional team are legally obliged to consider safety in all aspects of the design including the construction, usage and future demolition of the buildings. These considerations include the safety of the local community and the Principal Contractor will be responsible for the production of a 'Construction Phase Health and Safety Plan' that will include any safety measures that are required.

Walking and Cycling Accessibility

The proposal enhances the existing pedestrian and cycle access points to and from the site. There is good circulation for pedestrians and the works will complement the existing extensive cycle network.

Safe, secure bicycle storage will be provided at key points around the scheme with connections to the cycle network.

Bus Access

The site is located in a sustainable location with bus accessibility immediately outside the site on A6 London Road.

Cultural Diversity

Contractors and future operators will be required to have an 'Equal Opportunities' policy.

Employment

The proposed scheme provides additional employment opportunities both construction and long term operation of the site.

PROPOSALS

SITE LAYOUT

The proposal comprises:

	SQ FT	SQ M
Unit 1	33,023	3067.8
Unit 2	10,697	993.7
Unit 3	35,498	3297.8
Unit 4	10,550	980.0
Unit 5	21,450	1992.7
Unit 6	13,540	1249.5
Unit 7	5,475	508.6
Unit 8	4,287	398.3
Unit 9	4,757	441.9
Unit 10	5,398	501.5

Proposed Customer Car Parking: 320 spaces including 22 disabled spaces

Plus 5 motorcycle and cycle parking zones (m2/c)

	SQ FT	SQ M
Total proposed area	144,675	13440.3
Total area demolished	19,689	1829.1
Total existing area	115,693	10747.8
Additional area created	28,982	2692.4



CONCLUSIONS

Passfund Custodian Trustees Limited - acting on behalf of the Royal Mail Pension Plan - is a long term investor, committed to enhancing their investments and the surrounding community. For this site, they are firmly of the view that this site would best serve the community by providing additional good quality retailing together with a significantly enhanced local environment, that complements the City Centre, without prejudicing its vitality and viability.

The existing site has planning consent for additional units, floorspace and access. This statement supports the Reserved Matters for the proposal.

The proposed scheme will improve the setting of the surrounding Conservation Areas and the nearby Listed Buildings with the overall improved local environment and landscaping.

The rationalised parking and pedestrian areas will significantly improve the local environment and aid the legibility and ease of movement within the site, with the enhanced facilities and creation of a 'street' frontage for pedestrians.

The introduction of a new single access point and building to off set the existing Burger King will provide a 'gateway' into the site and create a better street presence.

The enclosure of the rear of the site and removal of two vehicle access points together with an enhanced CCTV provision will significantly reduce the opportunity for crime.

The appearance of the site frontage will reflect a more attractive, bright, contemporary look, with greater cohesion and legibility.

Overall the repositioned retail park will provide a revitalised anchor to London Road / Bocherigate area of Carlisle and facilitate increased prosperity to the local community.