

SCHEDULE A: Applications with Recommendation

09/0017

Item No: 13

Date of Committee: 29/05/2009

Appn Ref No:
09/0017

Applicant:
Mr M Doherty

Parish:
Beaumont

Date of Receipt:
14/01/2009

Agent:

Ward:
Burgh

Location:
Land At Field No 4490, Monkhill, Cumbria

Grid Reference:
334442 558905

Proposal: Change Of Use Of Agricultural Land To Holiday Accommodation
Comprising Of 2no. Self-Catering Cabins, 3no. Camping Cabins,
Services Cabin, 10no. Tent Pitches, Access Road, Alterations To
Existing Vehicular Access And Placement Of 1no. Dwelling For
Occupation By Site Manager (Revised Proposal)

Amendment:

REPORT

Case Officer: Colin Godfrey

Reason for Determination by Committee:

This application has been brought before the Development Control Committee based on the number of objections received and also as three objectors and a Ward Councillor requested their Right to Speak.

1. Constraints and Planning Policies

Ancient Monument

Gas Pipeline Safeguarding Area

The proposal relates to land or premises situated within or adjacent to the Gas Pipeline Safeguarding Area.

Local Plan Pol DP1 - Sustainable Development Location

Local Plan Pol CP1 - Landscape Character

Local Plan Pol CP4 - Agricultural Land

Local Plan Pol CP5 - Design

Local Plan Pol CP6 - Residential Amenity

Local Plan Pol EC16 - Tourism Development

Local Plan Pol H7 - Agric, Forestry and Other Occup. Dwgs

Local Plan Pol LE5 - Hadrian's Wall World Heritage Site

Local Plan Pol LE29 - Land Affected by Contamination

Area Of Outstanding Natural Beauty

Local Plan Pol DP9 - Areas of Outstanding Natural Beauty

2. Summary of Consultation Responses

Cumbria County Council - (Highway Authority): This is a revision of the scheme submitted under application number 08/1043.
The applicant has now however supplied additional information providing the actual vehicle speeds.
The views of this Authority remains the same with one change – the required visibility splay can be reduced from 2.4m by 120m to 2.4m by 70m.

I can therefore confirm that I have no objection to this application but would recommend that the following conditions are included in any consent you may grant:

The whole of the access area bounded by the carriageway edge, entrance gates and the splays shall be constructed and drained to the specification of the Local Planning Authority in consultation with the Highway Authority.

Reason: In the interests of road safety.

To support Local Transport Plan Policies: LD5, LD7, LD8

The development shall not commence until visibility splays providing clear visibility of 2.4metres by 70metres measured down the centre of the access road and the nearside channel line of the major road have been provided at the junction of the access road with the county highway. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (or any Order revoking and re-enacting that Order) relating to permitted development, no structure, vehicle or object of any kind shall be erected, parked or placed and no trees, bushes or other plants shall be planted or be permitted to grow within the visibility splay which obstruct the visibility splays. The visibility splays shall be constructed before general development of the site commences so that construction traffic is safeguarded.

Reason: In the interests of highway safety.

To support Local Transport Plan Policies: LD7, LD8

The use of the development shall not be commenced until the access has been formed with 4.5 metre radius kerbs, to give a minimum carriageway width of 4.8 metres, and that part of the access road extending 6 metres into the site from the existing highway has been constructed in accordance with details approved by the Local Planning Authority.

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Reason: *In the interests of highway safety.*

To support Local Transport Plan Policies: LD7, LD8

Access gates, if provided, shall be hung to open inwards only away from the highway, be recessed no less than 6.5m as measured from the carriageway edge of the adjacent highway and shall incorporate 45 degree splays to each side.

Reason: *In the interests of highway safety.*

To support Local Transport Plan Policies: LD7, LD8

Details of all measures to be taken by the applicant/developer to prevent surface water discharging onto or off the highway shall be submitted to the Local Planning Authority for approval prior to development being commenced. Any approved works shall be implemented prior to the development being completed and shall be maintained operational thereafter.

Reason: *In the interests of highway safety and environmental management.*

To support Local Transport Plan Policies: LD7, LD8

The use shall not be commenced until the access and parking requirements have been constructed in accordance with the approved plan. Any such access and or parking provision shall be retained and be capable of use when the development is completed and shall not be removed or altered without the prior consent of the Local Planning Authority.

Reason: *To ensure a minimum standard of access provision when the development is brought into use.*

To support Local Transport Plan Policies: LD5, LD7, LD8 and Structure Plan Policy: T32

Community Services - Drainage Engineer: awaiting comments;

United Utilities (former Norweb & NWWA): I have no objection to the proposal.

A water supply can be made available to the proposed development.

Should this planning application be approved, the applicant should contact our Service Enquiries on 0845 7462200 regarding connection to the water mains/public sewers

Please note that a copy of these comments has NOT been forwarded to the applicant/agent as the details were not supplied.

Environmental Services - Environmental Quality: with reference to the above planning application, I have the following observations.

1. With regard to the proposed cafe/kitchen the applicant should contact Mr M Neatis in the Food Health and Safety Team at the Civic Centre or by telephone on Carlisle 01228 817302 to discuss the food safety requirements
2. It will be necessary for a site licence for tented camping sites to be issued if the planning application is approved and implemented

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Cumbria County Council - (Archaeological Services): The site is archaeologically sensitive as the remains of a ditch of probable prehistoric origin were revealed in the archaeological evaluation that was carried out prior to the submission of the application.

The proposed development avoids the prehistoric ditch and the evaluation in the remainder of the site revealed no archaeological remains. I therefore have no recommendations or comments to make regarding the application. However, should the proposed development be amended in the future, I would be grateful if I was reconsulted;

Cumbria Constabulary, Northern Community Safety Unit: I have no observations or comments to offer in respect of this application

Local Plans (Tree Preservation), Development Services: Following our site visit on the 24 February 2009 I have the following comments/observations to make.

As previously discussed I have assessed the hedgerows adjacent the road and the track and they qualify as important hedgerows under the 1997 Hedgerow Regulations.

Having discussed the access on site I am satisfied that the requirements of the Highways Authority can be met by the trimming/cutting back of the hedgerow, and without the need for hedgerow removal.

The stone gate stoop should be re-used at the location of the new gate to be formed at the end of the proposed hard standing area.

Development Services Planning & Housing Services - Local Plans: There are two aspects to the proposed development: the use of the site as a campsite and the siting of a dwelling for occupation by the site manager. The proposal should be assessed against Policy EC16 of the adopted Local Plan in order to establish the suitability of the site and location for tourist accommodation. EC16 sets out six criteria against which tourism development should be assessed which relate to impact on landscape, accessibility, safeguarding of culture, heritage and environment and linkages to established tourist attractions. EC16 is also supportive of applications for tourism related development which aim to promote the enjoyment and understanding of Hadrian's Wall WHS, with the supporting text referring specifically to opportunities for the provision of new facilities close to the trail particularly refreshment facilities, toilet blocks, car parks and accommodation. The proposed scheme is for self-catering and camping style accommodation in the form of cabins and tents and associated facilities. It is intended mainly to meet the needs of walkers and cyclists following the Hadrians Wall Trail and C2C cycle route which run near to the proposed site. The principle of the proposals would therefore appear to comply with criteria 2,3, 4 and 5 of EC16.

In respect of landscape impact of the cabins and their impact on the Hadrians Wall buffer zone they are small-scale low level removable structures which could if required be removed from the site without any significant restoration of the land. The

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tent pitches will also have minimal impact. The plans show there to be minimal hard surfacing to accommodate parking/barbecue areas with the intended surfacing materials being designed to enable grass to grow through. The applicant has also included details of further screening of the site and cabins in addition to the existing hedgerows that will minimise any visual impact.

The second aspect of the proposal relates to the siting of a managers dwelling. The Good Practice Guide on Planning for Tourism states that for many types of holiday parks a residential managerial presence is often essential to achieve quality service to the customer, security for the property and to meet the obligations of health and safety. In order to assess proposals for staff accommodation the Guide suggests that consideration should be given to annex A in PPS7 which requires the applicant to meet the criteria set out in paragraph 12.

Policy H7 of the Local Plan provides guidance in respect of dwellings essential to agriculture, forestry and other rural based enterprises. This policy is based on the guidance in PPS7 annex A. As this proposal is a new enterprise it would be appropriate in line with PPS7 that in addition to meeting the criteria in paragraph 12 to grant a temporary consent for the managers accommodation for the first three years in order to demonstrate the financial viability of the camp site.

In addition it is important that the use of the manager's accommodation is tied to the use of the rest of the site as a camping facility. I would therefore suggest a condition to ensure that should the camping use of the site cease, the residential use of the manager's accommodation should also cease. You may also wish to consider the long term implications if the site ceases to be used as a camp site, and include measures for the removal of the structures.

In summary I consider that the principle of this development complies with the relevant policies within the local plan by supporting the Hadrians Wall National Trail by contributing towards the provision of a choice and range of accommodation types and facilities along the route of the trail.

Beaumont Parish Council: On receipt of the revised plans, the Parish Council following meeting and discussions with parishioners, of whom a very high proportion felt that this is an inappropriate development, would make the following comments and observations. The response is split into two parts, the original comments and the subsequent feedback from the consultation session, which took place due to the level of feeling on this proposal.

- The Design and Access statement refers to future development - to what extent would this be and are there any plans available for this;
- there is an ongoing issue with a drain at the side of the road and close to northern boundary of proposed development. The drain leaks what is apparently effluent - this could pose a health risk to the site;
- the road near Lock House has a tendency to flood - will the development exacerbate this issue;
- would the planning department place a condition requiring a return to agriculture if the project were to fail;
- would planning conditions be placed to constrain the development of the facility

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into a more general recreational park / caravan site, specifically restricting the operation to supporting the needs of the Hadrians Wall walkers on which this approval is based;

- there is already a camp site at Grinsdale Bridge and accommodation at both Vallum House Hotel on the outskirts of Carlisle and also in villages to the west - why do we need another so close;
- there is a feeling that a wigwam does not fit in with the surroundings;
- the issues of increased traffic has been covered by reference to 'use by walkers' the sites shown on the plans display parking provision on all of the wigwams, cabins and two of the tents. It must be remembered that this junction with Carlisle to Bowness road is dangerous especially access from minor roads;
- if consent were to be granted would it be on the understanding that the key workers house remains as such and is not subsequently used as a property for rent thereby allowing the site itself to become unsupervised;

Comments post consultation session;

- the general majority view was that this area could not be more rural, the very thing we are supposed to be protecting and the placement of this development was not appropriate;
- there was a minority view that rural areas do need investment;
- the comment on development was a question as to whether it would be constrained to the present boundaries of the field and not allowed to spread;
- the higher northern end of the site where the managers lodge will overlook Lock House due to its higher elevation;
- the site is much higher than the road at the north end which is where all the road floods. The soakaway would seem to be in receipt of all the site flow, which would normally have been slowed by natural percolation. The introduction of any hard surfaces including buildings must accelerate water flow, which can be an issue where soakaways are concerned. Whilst percolation tests have been done (were the percolation tests done in wet or dry conditions?) it is worth noting that the road has a tendency to flood already, A solution must be far from simple as our local road repair team seem unable to solve the problem; this has been subject to numerous complaints and attention to no avail. The distance from the northern end of the soakaways concentrated wet areas, to the northern boundary, does not seem far, it is difficult to see how the site will not exacerbate the present tendency of the road to flood at this point;
- great stress has been placed on the support of Hadrians Wall Heritage Ltd, without which it is very doubtful that this proposal or any other would get consideration. However, there is no supporting information beyond the letter from Hadrian's Wall Heritage Ltd, which would indicate the actual level of requirement for this facility. It would be useful to see this as part of the application;
- there is still a view expressed by those who have an interest in walking that the Carlisle to Bowness section of the wall is seen as one leg which is not completed in two sections. This facility is somewhat distant from the starting point at Carlisle which questions its proposed location;
- if planning approval is given and the venture fails, will the planning department give a clear indication at the outset, of future obligations to return the land to its original agricultural status;
- there is still a view that the Wigwam design, which has not been seen in the area

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- before, was not in keeping with the surroundings;
- the recent traffic survey took place in what was probably the quietest period of time of the year. During the summer month's farm traffic with its inherently large vehicles are much more in evidence. The current road is narrow and is taken up almost completely when occupied by the above mentioned vehicles, plus any busses. In addition there is very little room to walk on what are poor verges when this heavy traffic is passing. Currently there are not many if any walkers on this part of the road as the walking paths 'The Cumbria Coastal Path' which follows the river, and 'The Hadrian's Wall National Trail' which cuts across country from Beaumont to Burgh by Sands, do not use this section of the road, this would increase if walkers de-toured to the proposed site;
- Wildlife such as roe deer, foxes and partridges are seen moving through the area, the open corner area where this development is proposed could almost be seen as the only corridor to the wider countryside beyond, connecting the triangle of fields bounded by Kirkandrews-Upon-Eden, Beaumont and Monkhill. A concentrated increase in human activity at this point must have an impact on wildlife movement;
- it has been assumed that as this application is virtually the same as that which was withdrawn only a short while ago ref 08/1043. The previously personal comments posted by Parishioners to the planning department should be added to this planning request. If this cannot be done the said people will need informed as some did not realise it was necessary to write again;
- a site visit would be a necessity for this application;

The following further comments were received on the 14th April 2009;

- Whilst the application is made under the provision within Policy EC16 (covering support for tourism on and around the Roman Wall), the Parish Council believe this should not be interpreted as a blanket permission and that there should still be a requirement to provide the appropriateness of any given application. We feel that supporting documentation should cover the actual numbers which would be expected, with the source and support for these numbers clearly stated. During the course of the planning meeting the number of 14500 people was mentioned as users of the walks. With parishioners living adjacent to the village green, which is used as a resting point, we can assure Committee that no such numbers are experienced. As an indicator the village waste bin on the green is emptied only once every two weeks in high season;
- On the day of the meeting it became known that the site was already being advertised as a rural destination away from the city. In this planning application the site is being presented as a resting place for hikers and walkers, cars were not seen as a significant part of the equation. We would suggest that such an advertisement would indicate a degree of traffic beyond that presently presented in the application could be anticipated. Bearing this in mind does the present layout reflect this and should the highway department be asked to re-assess the situation, especially as the planning committee experienced at first hand the ease with which congestion occurs and this is at the quiet time of year;
- The plans for this application do not show sections through the field which in reality is elevated some way above the adjoining road, anything which

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accelerates or concentrates the flow of water as would the provision of soak-away served washing, toilet facilities and laundry provisions, must exacerbate the current flooding problem;

- It is worth noting that the area of the road suffering flooding is in the direction of the walks and will have to be traversed by any users of the site, this will force people away from the heavily flooded verges into the shallower water in the centre of the road. As the road bends sharply at that point oncoming vehicles are also in the centre of the road as they try to avoid the deeper water thus exacerbating the associated safety issues;

Hadrians Wall Heritage Limited: I am writing to you, as the Hadrian's Wall Path National Trail Manager, with some additional information in support of Martin Doherty's development of Monkhill Camping Park.

First of all I can confirm that there still exists a general walkers' complaint about a shortage of short-stay accommodation west of Carlisle, in particular for camping/hostel and budget B&Bs. The situation since the Trail opened in 2003 has improved elsewhere, to the point whereby I am more or less happy with the accommodation provision, with the area west of Carlisle being the last to catch up.

It may be helpful to quote from Natural England's 2007 national survey of all of England's National Trails. The survey revealed that 68% of long-distance walkers using National Trails (of which Hadrian's Wall Path has 7,000, plus several thousand more short-stay visitors) use either B&Bs or camping sites/camping barns/hostels.

The survey also confirmed that long-distance walkers, travelling on foot, quite understandably prefer to see more and cheaper accommodation close to the Trails. Without access to cars walkers also want to stay as close as possible to the route and I would say that Monkhill Camping Park helps to meet that aspiration.

I would also like to highlight the fact that Hadrian's Wall Path is attracting a sizeable constituency of people who are new to walking in the countryside; this can only be a good thing but it is clear from my postbag that these people have their own particular requirements, many of whom prefer not to carry their heavy packs with them whilst on the Trail. This is reflected in the fact that Hadrian's Wall Path now supports three full-time baggage carrying companies who transport walkers' bags every morning between accommodation addresses.

To summarize, most National Trail walkers using Hadrian's Wall Path do not have access to cars, they arrive and leave on foot each day and they rely on local accommodation provision and supporting services to enable their holidays to become a reality. There remains a shortage of budget accommodation, including camping provision, west of Carlisle, and I believe that the Monkhill Camping Park development will help to meet that need.

Environment Agency (N Area (+ Waste Disp)): The Agency has no objection to the proposed development provided that any approval includes the following planning condition:

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CONDITION

Prior to the commencement of development approved by this planning permission (or such other date or stage in development as may be agreed in writing with the Local Planning Authority), the following components of a scheme to deal with the risks associated with contamination of the site shall each be submitted to and approved, in writing, by the local planning authority:

- 1) A preliminary risk assessment which has identified:
 - all previous uses
 - potential contaminants associated with those uses
 - a conceptual model of the site indicating sources, pathways and receptors
 - potentially unacceptable risks arising from contamination at the site.
- 2) A site investigation scheme, based on (1) to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.
- 3) The site investigation results and the detailed risk assessment (2) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.
- 4) A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in (3) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action.

Any changes to these components require the express consent of the local planning authority. The scheme shall be implemented as approved.

REASON

The information provided with the application is inadequate and needs to be supplemented with a desk study as per the guidance in CLR11. This is required to protect the quality of Controlled Waters

AGENCY INFORMATIVE

Formal consent of the Agency will be required for the discharge of treated sewage effluent to soakaway, under the Water Resources Act of 1991.

The applicant is advised to contact the Environment Agency's Permitting Support Centre, P O Box 4209, Sheffield. S9 9BS (Tel No. 08708 506 506) to discuss the matter in detail

Following discussions with the applicant, the following revised response has been received;

I have spoken to the applicant regarding the above today. I believe that this resubmitted application defined the previous use as "agricultural" contrary to the

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original application (08/1043) .

I have spoken to our contaminated land officer Peter Bardsley today and we have agreed that in the light of this, and the previously submitted information, the contaminated land condition requested by the Agency on 09/0017(see e-mail 26/01/09) can be removed.

English Heritage - North West Region: Although this site lies within an archaeologically sensitive area, between the line of Hadrian's Wall and the Hadrian's Wall vallum, in our view its nature and carefully considered location is such that it will not have an adverse impact on the setting of the Wall in a way that impacts on the Outstanding Universal Value of the World Heritage Site.

With reference to direct impact of the proposal on archaeological remains, the applicant has undertaken an archaeological evaluation of the site and which although it did discover ditch features of probable Bronze Age date, did not reveal any archaeological remains relating to Roman frontier. As such, English Heritage is happy to defer to the County Archaeologist as to the appropriate archaeological mitigation, and provided their advice is followed we would not wish to sustain an objection to this application.

Northern Gas Networks: UU has no objections to these proposals, however there may be apparatus in the area that may be at risk during construction works and should the planning application be approved, then we require the promoter of these works to contact us directly to discuss our requirements in detail. Should diversionary works be required these will be fully chargeable.

3. Summary of Representations

Representations Received

Initial:	Consulted:	Reply Type:	
Road	26 Skiddaw	20/01/09	Objection
Northview Farm	20/01/09	Objection	
	20/01/09	Objection	
, Riverview			
Santor	20/01/09	Objection	
Bushy Bank	20/01/09	Objection	
Ivy Cottage	20/01/09	Objection	
Yeavinger	20/01/09	Objection	
The Old Windmill	20/01/09	Objection	
Mill Farmhouse	20/01/09	Objection	
Beaumont House	20/01/09		
Monkhill	20/01/09		
Braelees Cottage	20/01/09	Objection	
Harrys	20/01/09	Objection	
Cuswin	20/01/09	Objection	
Glenstrae	20/01/09		
, Gracelands	20/01/09	Objection	

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Linsum	Millfield	20/01/09	Objection
	Castle Cottage	20/01/09	Petition
	8 Longburgh Fauld	20/01/09	
	Loughrigg	20/01/09	Objection
	Friars Garth	20/01/09	Objection
	Ridge Lea		Objection
	Lock House		Objection
	Hollow Creek Farm		Objection
	Greenstones		Objection
	- Burgh by Sands		Comment Only
	, Mylen House		Petition

- 3.1 This application has been advertised by means of a site notice as well as notification letters sent to 22 neighbouring properties. In response, 21 letters of objection have been received and are summarised below;
1. no significant need as sites at Geltside and Grinsdale Bridge;
 2. security - could be up to 60 people if campsite is full;
 3. no amenities in surrounding area;
 4. this will cause noise and change the appearance of an old part of Carlisle area between Monkhill and Beaumont;
 5. extra traffic;
 6. the small B roads around Beaumont and Monkhill are not suitable to cater for the proposed increased in road traffic. As it is when two cars pass each other one has to pull on to the verge to allow the other to pass, cutting up the verge and pulling all the mud onto the road;
 7. increased traffic will increase noise in the adjoining villages;
 8. the proposed wigwams and buildings will not sit in harmony with the village as they will be out of keeping with the existing houses, to the detriment of the World Heritage Site;
 9. Safety is a concern both from the increase in traffic but also from the steady stream of this new transient population potentially making use of the site. Currently the beauty of Beaumont is that only people accessing Beaumont itself go there, now however a steady stream of strangers will be accessing through the village to stay at the site;
 10. although the developers are saying they are aiming at folk who are wishing to walk the wall, they would be powerless to prevent people who wish to use the site to 'party' thus increasing noise in the area;
 11. a campsite exists 2 miles away from Kirkandrews at the car sales bridge, thus the grounds for this development are already catered for;

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12. the proposal indicates that the site could grow in the future which amplifies all the previous concerns by another factor of magnitude;
13. the proposed use of the land is so out of keeping with current use that Wigwams, buildings and a shop will change the village environment forever, for the worse;
14. a much better solution would be to site this proposal well out of the way of the local community where it could exist with good access and not detract from our village, community and wildlife;
15. this is a greenfield site and it should not be used for any other use than agriculture;
16. the proposed development would increase traffic flow at an already dangerous crossroads;
17. is the application for timber (temporary type) buildings just a way of gaining planning permission, then when they have passed their sell by date they will be replaced by permanent homes;
18. if permission were to be granted then the field either side could risk being developed as these could then be classed as in-fill plots. This would join both the villages of Monkhill and Beaumont together creating one large developed area rather than two small quiet tranquil villages;
19. there are no amenities in Monkhill, the pub has closed again having only been open for approx three months prior to this it was closed for approximately eight months. The bus service is limited;
20. it is a green field site and should not be used for new development when accommodation for walkers already exists along this section of the route and so close to Carlisle;
21. it will have a significant effect on local wildlife, especially roe deer, who use this and adjoining fields as a means of passage and pasture;
22. access in and out of the proposed site will create a safety issue to passing vehicles as the site entrance is very close to a blind bend;
23. this is a very narrow road from Monkhill to Beaumont and pedestrians and young children who walk this route have to stop and give way and climb onto the grass verges to avoid traffic and buses. This is dangerous in itself as the verges are constantly being severely damaged by large vehicles which can't pass each other without mounting the verges;
24. the proposed development will certainly create a greater hazard than already exists in vehicles gaining access to the Monkhill / Burgh Road crossroad;

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25. there is concern over the septic waste from this proposed development. The planned soakaway into land that constantly floods onto the road from this field and others so much so that it is impassible at times for pedestrians and dangerous for vehicles. It is felt that health and safety will be compromised;
26. concern over noise, litter, day and night disturbance in this peaceful area;
27. an increase in seasonal visitors raises concerns regarding increased security risk;
28. The Supporting Planning Statement refers at 4.4.6 to a preference to find local accommodation, and states "where this is not feasible the guidance does allow for the principle of provision for on - site accommodation." In this case, however, it is perfectly feasible to obtain suitable accommodation. There is at this moment a modestly priced property for sale within walking distance of the proposed site;
29. the case for lack of feasibility appears to be based upon the fact that the applicant lives in Maryport, and the scale of the proposed business would not sustain the purchase of a house. This is no doubt true, but spurious. Everyone has to live in a house of some kind, and its value is normally irrelevant to any business which the resident might undertake. There is nothing to prevent any applicant from relocating to say, Monkhill. If he were unable to afford to do so it would be unfortunate, but this would not make a case for on-site accommodation;
30. The fact that Monkhill is referred to as a sustainable development location at DP1 is irrelevant as the proposed site is outwith the village;
31. if granted, this dwelling would constitute the thin end of a wedge. If this application were to be granted it would be difficult to refuse a further application for a residence 'only 215m' away from this one, leading ultimately to infilling the countryside between Monkhill and Beaumont;
32. The applicant seeks to quell fears by referring to the business being tied to the business, drawing attention to the fact that the occupancy would cease if the business should fail. The fear is not that it might fail, but that it might succeed beyond all expectations, leading to almost inevitable expansion;
33. In the statement at 4.1.1. 'is anticipated that the majority...will arrive by foot or bicycle'. We can only conclude that this statement was made with tongue firmly in cheek. The very strong likelihood is that they would arrive by car, no doubt with bicycle and/or climbing boots, and use the site, with its convenient parking, as a base. This would involve unacceptable increase in traffic on a very narrow road, and particularly at the Crossroads at Monkhill, which has appallingly poor visibility to the left when emerging from the direction of the site;

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34. At 4.4.9, the services provided by the cabin (Cafe etc.) would be 'extended to passing walkers and cyclists'. Motorists are not mentioned but it would obviously be impossible to distinguish a motorist from a walker. If this Cafe were to prove a success, impossible congestion in the vicinity would result, no safe roadside parking being possible;
35. the width of the road serving the site is extremely narrow - two cars can't pass at the same time. It is also used by the local bus service, milk wagon's, heavy agricultural vehicles and cattle;
36. the ditches to the side of the road are badly damaged and the gulleys are blocked and the road floods badly. This proposal will only add more problem's to the Monkhill to Beaumont Road. There is also no street lighting or road markings;
37. there are no amenities in Monkhill;
38. the field is agricultural land and should be kept so;
39. the site is a bronze age site close to the Vallum. Our heritage should be protected, not developed upon;
40. a camp site would be noisy, create extra traffic and possibly invite crime. Monkhill and Beaumont consists of a lot of elderly residents;
41. there are existing tourist facilities in the local area;
42. this proposal would add extra traffic to narrow busy roads;
43. the drain that's positioned just past the proposed site leaks effluent and it has done for the past thirty years;
44. it is understood that applicant resides at Crosby near Maryport, as such, it is considered that a site in Maryport would have made more sense as there is a lot more interest and history there, for example, a Roman museum;
45. discussions with walkers do not suggest that they require a campsite between Beaumont and Monkhill;
46. if the proposal went ahead and was not successful, would the cabins and the dwelling be removed and the field re-instated?
47. the seven additional permanent buildings, in a small rural community would represent a substantial development, and will potentially change the character of the area;
48. although there is a 30mph speed restriction in operation we have problems with speeding traffic entering the village. This development would lead to more traffic and hence make this problem even worse. Also,

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the single track roads around Beaumont attract walkers and cyclists and the extra traffic would potentially make conditions for these road users more hazardous;

49. the change of use would be incompatible with the existing environment, this is a rural district on the edge of an Area of Outstanding Natural Beauty and would not benefit from the kind of development proposed.
50. it is located within an isolated community with no amenities;
51. there is little or no public transport to or from Carlisle outside of daylight hours and users of the facility would therefore be tempted to make their own entertainment with all the consequent noise and light pollution for nearby residents;
52. the proposal is economically flawed and stands a good chance of failing, in which case the investors would be tempted to recoup their investment by letting the cabins on a permanent basis, with all the consequent social drawbacks;
53. it is agreed that the Solway needs more tourist accommodation, but not the low grade sort envisaged in this application. There are plenty of camp sites in the area, and in any case real campers pitch their tents in convenient fields. What is needed is more good quality bed and breakfast accommodation and restaurant facilities. The proposal does nothing to encourage this. On the contrary it would set an unwelcome precedent that could result in the Solway being surrounded by low grade developments of this sort. This would discourage tourists from visiting our area. If we are to encourage British tourists who currently holiday abroad to come here we have to offer more than a wigwam in a field. They will demand good quality accommodation, e.g. en-suite facilities as a minimum;
54. the proposed site is outside the village of Monkhill but still unfortunately close enough to impact it. If this site were developed it would then create a possible application to in-fill between the Chapel and the campsite. And if permission has been granted for this site outside the village who knows how long it would be before Beaumont and Monkhill become one village corridor!;
55. the size of this proposed campsite which the developer continually refers to as small is not small enough. In the notes it says the septic system is designed for 25 people but if there are 3 wigwams (for 2 people) and 2 self catering cabins (may sleep more than 2) and then 10 tent sites (could be for 2 people or more) as well as a site managers house then that adds up to more than 25 people even without it being full. Considering the village of Monkhill consists of around 32 houses this would seem like a large increase of population for this area during the summer months;
56. why build a site managers house when there is a perfectly good house for

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sale in Monkhill currently;

57. The road network around Monkhill and Beaumont is not suitable for the type of traffic that this sort of development would bring to the area e.g. cars and caravans and the large vehicles needed to service this type of development;
58. Surely the damage to the local area and the increase from traffic will go against everything we as a County are trying to achieve;
59. Trying to save our local landscapes especially in an area like the Solway Coast must be of the up most importance because as we know from other areas around the Country once we lose these areas of outstanding natural beauty we can never get them back sadly a situation where we have only looser;
60. As we know these sites do not stay small and once started rarely stop until they become a blot on the landscape just travel to Silloth for an example of this and as the application also proposes a dwelling for a site manager it is hard to see how it can be financially viable to do this with a plane for a couple of cabins and ten tents suggesting they would look to expand the operation in the future and as previously stated it is easier to expand once the damage is done. I have no problems with someone wishing to build a family home in the area but this is just an ugly blot that I'm sure not many people would wish to view from their home;
61. There is no benefit commercial or financial to the local economy that I can see from this proposed development as we live in times where anyone staying on this development will just drive back in to Carlisle to do there shopping at one of the national chain supermarkets increasing traffic even more and damaging the environment even more;
62. Moving to this area was a decision made based on the fact that it was a quiet rural environment to bring our children up in, a development like this will ruin this beautiful area and as stated before once gone you can never get it back. Surly with the current financial climate the world finds itself in there are enough Hotels, Bed and Breakfast, Cottagers, Lodges and Camp sites struggling to fill the space they have available and make their business financially viable and keep their heads above water. This development will just create competition for an already struggling sector of our economy;
63. the campsite location is not actually on the Hadrians Wall trail unlike the many other B&Bs, campsites and bunkbarns in close vicinity to Monkhill;
64. Monkhill is a small rural hamlet with 32 dwellings and no local amenities - the local Public House has recently closed;
65. the bus route serving the area is limited;

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66. the staggered cross roads at Monkhill are difficult to negotiate at the best of times. Visibility towards Burgh-by-Sands is good. However, visibility towards Kirkandrews on Eden is exceptionally poor;
67. the proposed development intends to build on a site which is currently agricultural land. The land should remain for agricultural use. Surely Carlisle City Council should be promoting conservation of agricultural land and preservation of all animals which live and hunt on that land;
68. the development would spoil the countryside between Monkhill and Beaumont;
69. the ground is always wet even in summer on the Monkhill side of the proposed development, and on the Beaumont side the road floods on a regular basis even after moderate rain;
70. there is obviously a problem with the drains in that area and the development would compound the problem;
71. any development whatsoever in this green field would be a disaster. The small villages of Beaumont and Monkhill are separated by only four fields and this would create an in-fill situation encouraging other local developers to also destroy the farmland with more developing, causing, eventually in time, for Beaumont and Monkhill to merge into one.
72. we do not walkers trooping up and down the village looking in windows and gardens;
73. the countryside must be preserved for future generations and wild animals;
74. there is no necessity for any input into the local economy which would only consist of a few walkers visiting Monkhill pub, it has sufficient local trade, as a community to survive, only being closed at present due to wrongly placed tenants. We do not want to be imposed upon by the tourist trade and certainly do not want a cafe on the doorstep;
75. the proposal will prevent frogs and toads using their migratory route;
76. the applicant(s) claim an average site usage of 9-15 persons, but concede that 'there will be times when the numbers are outside this range'; the site is expected to be operational outside of standard tourist seasons, 'we hope to add extra cabins once we have gained enough income' and 'we hope to add an additional WC when an information point is added'. Clearly financial necessity will drive this project to operate at maximal capacity for as long as possible each year and the safeguards for the local area are not adequate to cope with this level of usage. They claim the site is mainly for walkers and cyclists but they cannot, and will not, discriminate against customers who arrive by other modes of transport, thus affecting traffic volumes;

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77. the applicant(s) answer to concerns regarding noise and crime is 'most people who walk Hadrian's Wall are over 30' - criminal and antisocial behaviour is not solely the preserve of the young;
78. I note 'the cabins are specially designed and used in remote areas of Scotland'. Whilst this may be a statement of fact, Beaumont is not remote. Furthermore, these cabins look absurd and out of keeping in Scotland, just as much as they will if allowed in to Beaumont / Monkhill. The applicant(s) claim that the 'height of the existing hedgerow will be increased by about 1m so the site will be unlikely to be visible'. However, a well managed more substantial hedge will take time to grow. In the interim, the site will be an eyesore;
79. the applicant(s) state that 'there is a pub in Burgh-by-Sands, only a 20 minute walk away'. They fail to point out that there is also accommodation in Burgh, very near to the pub. Why then would you wish to camp in Beaumont and walk 20 minutes to the pub, when you could stay in Burgh and only walk 5 minutes to the pub?;
80. this project begins the infill of the green space between two sperate hamlets - part of the beauty and character of the area. If this proposal is passed you the planners, and the Maryport applicants, will be responsible for the development of an indistinct urban dormitory, in place of green fields and open countryside;
81. it appears that the holiday accommodation will be available for use on a year round basis and must be judged in this context;
82. the siting of a dwelling in open countryside for use by the Site Manager directly conflicts with local plan policy. There is nothing in the documentation that we have seen to support an identified and proven need for a dwelling. In our view there must first be a need to justify the planning consent. To approach the issue the other way round would be entirely wrong;
83. there will be many months during the year when the holiday facility proposed will be barely used. If the facility is under used or worse still fails completely the precedent for a residential dwelling free of ant restriction will have been established. We would be grateful if in the Planning Officer's report to the Committee it could be clearly indicated how it is intended that the residential dwelling might be linked to the remainder of the development to prevent it being sold off or occupied separately from the development;
84. while we are aware that a traffic statement has been prepared its contents do not alleviate or eliminate our client's concern regarding the additional traffic that would be generated along this minor route. Existing traffic already has a serious and damaging effect upon the road verges and gutters. Photographs are enclosed illustrating the concern;

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85. the road network surrounding land in this area frequently floods and again this is clear from the photographs. It is a logical and understandable fear on our client's part that increased commercial and vehicular activity in the area can only make matters worse;
 86. Part of the development site proposed is elevated some 2 metres above the road and our client's adjoining land and property. He will be overlooked which will directly impact on his privacy and amenity. A development which adversely affects the amenity of neighbouring residential property is in direct conflict with local plan policies. Our client is aware that certain landscaping / mitigating measures are proposed in the form of planting. Any planting scheme will take years to mature and will at best only be effective during the summer months. The proposal in our view represents unneighbourly development;
 87. the development represents development in the open countryside. The thrust of national and local guidance is that the countryside should be protected for its own sake and where possible enhanced. The use of the site for the development proposed directly conflicts with this guidance;
 88. the land floods - the proposed access is at the junction of two beck's. One is a drain at the roadside from Monkhill, the other is a drain along the nearest field to the proposed entrance. The lonning is currently liquid mud;
 89. the proposed drainage system will have nowhere to empty;
 90. on inspection there is evidence of sewage contamination in stagnant gully pools which transfers to a much wider area under flood conditions. From a public health perspective to suggest people should live in, or adjacent to, these conditions is Dickensian. This leads me to speculate as to how long before an accident or outbreak of disease occurs;
 91. it has been suggested that this development would be good for local economy. Monkhill and Beaumont do not want any boost to our economy. We are two sleepy retirement villages, within commuting distance to Carlisle, where the few working people make there living before returning to these quiet little villages. The only economy that would be boosted by this house and development would be the applicants. Boosting the local economy would lead to growth of these small communities which is something we don't want;
 92. although the plan shows parking for the tents and cabins, no parking is provided for people visiting the snack bar;
- 3.2 In addition to the individual letters of objection, a petition signed by 45 residents of Beaumont has also been received. The petition does not provide details of the specific reasons for the objection;

3.3 A further petition signed by 32 residents of Monkhill has also been received. Again, the petition does not provide details of the specific reasons for the objection;

4. Planning History

4.1 In 2008, under application reference 08/1043, an application for the change of use of agricultural land to holiday accommodation comprising of 2no. self-catering cabins, 3no. camping cabins, services, 10no. tent pitches, access road, alterations to existing vehicular access and placement of 1no. dwelling for occupation by site manager was withdrawn;

5. Details of Proposal/Officer Appraisal

Introduction

5.1 Members will recall that they resolved to refuse permission for this proposal at the Committee Meeting on 24th April 2009. However, the decision was made in the absence of all the available information, particularly in relation to comments received from Hadrian's Wall Heritage Ltd. It was therefore considered, in fairness to all parties, to re-present the application at this meeting. As Members will be aware, the application seeks full planning permission for the provision of tourist accommodation and associated facilities within a field located on the eastern side of the road leading north from Monkhill to Beaumont. The application site is approximately 500m to the south-west of the Hadrian's Wall Path National Trail (HWPNT) within the Hadrian's Wall World Heritage Site Buffer Zone and adjoins the Solway Coast Area of Outstanding Natural Beauty. The field has an approximate area of 0.44 hectares and is bounded to the south, west and east by natural hedging. The northern boundary is a post and wire fence. The field is above the level of the road that runs along its western flank. Agricultural fields are located to each other flank with the nearest residential property, Lock House, located approximately 60m north-west of the northern boundary of the site. The closest properties within Monkhill are approximately 180m from the application site; the closest properties in Beaumont are approximately 200m. Access to the site is from the south-west corner of the field.

The Proposal

5.2 This application seeks approval to provide 3 camping cabins, 2 self catering cabins, a service cabin, storage shed, info cabin, ten tent pitches and a cabin to provide accommodation for a "key worker" within the site. It is also intended to provide a gravel access road and undertake landscaping, primarily through hedge planting. The site plan indicates that the proposed tents will be located on the eastern side of the access road (away from the public highway) and the remaining timber structures located to the west of the access road. The applicants have indicated that surface water drainage will

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be in the form of a soakaway with foul sewerage dealt with by means of a package treatment plant.

- 5.3 The proposed self catering cabins are to have a length of 8.1m (including overhang), depth of 2.7m, maximum roof height of 2.95m, and are to be constructed from timber. The cabins will provide a bedroom, toilet/shower and combined living space and kitchen.
- 5.4 The camping cabins are 'A' shaped buildings with a floor area of 5.5m x 3.7m and a roof height of 2.8m. The cabins will again be constructed from timber and provide a single living space with no washing facilities.
- 5.5 The service cabin is to have a floor area of 9.2m x 6m with a maximum roof height of 3.5m. It is to be divided into two distinct areas, one comprising a kitchen and café area; the second two toilets (including one suitable for disabled people) and two showers. A ramp is to be provided to allow access to the cabin for disabled people. The cabin is to be constructed from timber. Adjacent to the service cabin it is proposed to erect a timber drying room with a floor area of 3.63m x 1.82m and a roof height of 2.27m.
- 5.6 As noted, the tent pitches are to be located to the east of the site with individual pitches screened from one another by means of willow/hazel hurdles of approximately 2m in height. A dogwood hedge is to be planted to the rear of the tent pitches to provide delineation with the soakaway that is to be located directly behind.
- 5.7 The key worker's accommodation, that is for the manager and his/her family, is to be located at the northern end of the site. It is to be single storey with a floor area of 10.9m x 6m (maximum 13.1m x 7m including verandah). It will provide three bedrooms, a bathroom, toilet/shower and combined kitchen and living area and is to be constructed from timber.
- 5.8 The application is supported by a planning statement, design and access statement, traffic assessment, hedgerow survey report, archeological evaluation, and a business plan. The intention is for the proposed accommodation to be primarily, but not exclusively, used by those walking along the HWPNT and cyclists on the Coast to Coast (C2C) route. The facility will offer tourist accommodation throughout the year. Given the nature of the anticipated users, it is likely that the length of stay will generally be short, i.e. one or two nights.

Assessment

- 5.9 The relevant policies against which this application is required to be assessed are Policies DP1, CP1, CP4, CP5, CP6, EC16, H7, LE5 and LE29 of the Carlisle District Local Plan.
- 5.10 The proposal raises two distinct issues. The first is whether the principle of providing holiday accommodation in this location is acceptable and is considered at 5.11 to 5.31. The second relates to the acceptability of

providing key workers accommodation on site which is considered at 5.32 to 5.34.

1. Whether The Principle Of Tourist Accommodation Is Acceptable In This Location

5.11 The proposal falls outside of a sustainable development location as defined by Policy DP1 of the Carlisle District Local Plan. However, Policy EC16 of the Local Plan states that proposals for tourism related development will be supported where they contribute to the economic and physical regeneration of an area provided that the following criteria are achievable on site:

1. the scale and design of the development are compatible with the surrounding area; and
2. there would be no unacceptable adverse impact on the landscape/townscape; and
3. adequate access by a choice of means of transport, including sustainable modes of travel such as cycling or long distance walking and appropriate car parking can be achieved; and
4. the level of traffic generated can be adequately accommodated within the local road network without detriment to the particular rural character of the area; and
5. If the proposal is within a rural area it is well related to an established tourist attraction or an existing group of buildings, or would form an important element of a farm diversification scheme; and
6. the distinctive environment, culture and history of the area are safeguarded.

5.12 In addition, the policy stresses the importance of Hadrian's Wall as an attraction for sustainable tourism with the supporting text referring specifically to opportunities for the provision of new facilities close to the trail - particularly refreshment facilities, toilet blocks, car parks and accommodation.

2. Impact On Landscape Character

5.13 As noted, criteria 1 and 2 of Policy EC16 relate to the impact on landscape character and existing settlements. The proposal will result in the introduction of a number of timber structures on an open field within the Hadrian's Wall Buffer Zone and adjoining the Solway Coast AONB. A number of objections have been received on the basis that the proposed structures will be visually intrusive, especially in the context of their proximity to areas of recognised historic and landscape value. However, the cabins and associated buildings are small-scale low level removable structures which could, if required, be removed from the site without any significant restoration of the land. The tent pitches will also have minimal impact. The plans show there to be minimal hard surfacing to accommodate parking/barbecue areas with the intended surfacing materials designed to enable grass to grow through. The applicant has also included details of further screening of the site and cabins in addition

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to the existing hedgerows that will minimise any visual impact. It is therefore considered that the requirements of the aforementioned criteria are achievable on site.

3 . Accessibility

- 5.14 Criteria 3 of Policy EC16 requires any tourist facility to be accessible by a choice of means of transport. In this regard, the applicants are specifically marketing the site at walkers and cyclists following the HWPNT and C2C. As such, it is likely that a higher percentage of people using the facility would arrive on foot or by bike than would normally be associated with a development of this type. In addition, there is also a bus service which runs in close proximity to the site. Whilst it is inevitable that some people using the site will arrive by car, it is considered that the proposal provides sufficient alternatives to demonstrate compliance with criterion 3.

4. Impact On The Local Highway Network

- 5.15 The fourth criterion seeks to ensure that the level of traffic generated can be adequately accommodated by the local highway network. As already discussed, it is considered likely that a high percentage of visitors to the site will arrive on foot or by bike. However, the proposal will also generate additional motor vehicle movements. A number of concerns have been raised by residents in relation to this aspect of the proposal. Firstly it is asserted that due to the narrowness of the road between Beaumont and Monkhill, two vehicles - especially when this involves a tractor, bus or similar - are unable to pass without driving onto the verge. Photographs have been provided by one objector to show the damage which has already been caused to local verges as a result of this. Secondly, there are highway safety concerns because the staggered cross-road in Monkhill is considered to have particularly poor visibility. Likewise, the visibility for cars leaving the site is felt to be particularly poor. There is also a general concern in relation to increasing the amount of traffic on the relatively narrow rural roads in the vicinity of the site.

- 5.16 In relation to the impact on the verges, as demonstrated by the photographs submitted by the local resident, this is a pre-existing issue which, given the modest nature of the tourist facility and its emphasis on attracting walkers and cyclists, is considered unlikely to significantly exasperate the problem. In relation to safety concerns, the Highway Authority has raised no objection to the proposal subject to appropriate conditions being attached to any permission that may be granted to ensure, amongst other things, appropriate visibility splays and entrance to the site is achieved. On this basis, it is considered inappropriate to recommend refusal of the application on highway grounds.

5. Whether The Proposal Is Well Related To An Existing Tourist Attraction

- 5.17 Criteria 5 of Policy EC16 of the Local Plan states that proposals for tourism related development in the rural area must be *'well related to an established tourist attraction or an existing group of buildings, or would form an important*

element of a farm diversification scheme'. In this instance the proposal is clearly well related to the Hadrian's Wall World Heritage Site and the associated Hadrian's Wall Path National Trail. As such, it is considered that the requirements of this criteria are met.

- 5.18 A number of objections have been received on the basis that there is no need for the facility as there are numerous other establishments of various kinds offering accommodation for people visiting / walking the Wall in the wider area. However, Hadrian's Wall Heritage Ltd have provided a letter of support for the proposal which states that *'we are particularly interested in the development due to the fact that it will offer high quality, environmentally friendly facilities for walkers and cyclists in an area of Hadrian's Wall corridor that currently has no provision of this nature'*. In addition, a further letter of support has been received from Hadrian's Wall Path National Trail Manager which states, in summary, that *'most National Trail walkers using Hadrian's Wall Path do not have access to cars, they arrive and leave on foot each day and they rely on local accommodation provision and supporting services to enable their holidays to become a reality. There remains a shortage of budget accommodation, including camping provision, west of Carlisle, and I believe that the Monkhill Camping Park development will help to meet that need'*. In addition, the applicant has submitted a publication entitled 'The Economic Impact of Cycle Tourism in North-East England' which has been produced by Sustrans. This publication states that 160,000 trips were made on Hadrian's Cycleway in 2006 (79,000 in the North-West region). Of these, 7500 were end to end users. The route generated £6,500,000 and created or safeguarded 105 full-time equivalent jobs.

- 5.19 It may be asserted that granting approval for this development could potentially impact on the financial interests of nearby enterprises - listed by objectors - which also provide tourist accommodation, albeit of a different kind, in close proximity to the Wall. However, ministerial advice has often re-iterated that considerations of commercial competition are not planning matters and this would therefore not provide a valid reason for refusing the application.

6. Whether The Proposal Would Safeguard The Local Environment

- 5.20 The final criteria of Policy EC16 requires that *'the distinctive environment, culture and history of the area are safeguarded.'* As noted, the proposal falls within the buffer zone of the Hadrian's Wall World Heritage Site and is therefore in a location of acknowledged historical significance. Concern has been raised that the proposal will have a detrimental impact on the historic and archaeological interest of the area. As such, the applicants have submitted an archaeological evaluation of the site and both the Cumbria County Archaeologist and English Heritage were consulted. However neither raised any objection to the proposal as it was not considered that any archaeological remains would be affected.

- 5.21 In addition to impact on the historic environment, concern has also been raised as to the potential impact on wildlife. However, it should be noted that

the proposal falls within an intensively managed agricultural field which has most recently been used for grazing animals. As such, the field is likely to have low intrinsic value for wildlife. Whilst it may be used by foraging animals, the field represents a small part of an extensive resource of similar habitat and any impact on wildlife is therefore likely to be minimal.

- 5.22 In addition to the above, should permission be granted, the proposal would require the hedgerow around the proposed entrance to be cut back to allow the visibility splays required by the Cumbria Highway Authority to be achieved on site. It may also be necessary to remove a small amount of hedgerow (<0.50m) to allow the minimum carriageway width of 4.8m to be achieved. The Council Tree Officer has assessed this hedgerow and has advised that it would qualify as 'important' under the terms of the Hedgerow Regulations 1997. As such, he has noted that there is strong presumption in favour of it's with any loss only warranted in exceptional circumstances. However, given the minor nature of any hedgerow removal which may be required and the mitigation afforded by additional planting within the site, it is not considered that there would be sufficient grounds to warrant refusal of the application on the basis of the loss of this small section of hedgerow, if required.

- 5.23 In addition to the above, a number of additional concerns have been raised in relation to the tourist accommodation which are addressed below.

7. Crime and Anti-Social Behaviour

- 5.24 A number of residents have raised concerns that by potentially having up to 60 tourists in the area at one time, there will be an increased risk of criminal activity within Beaumont and Monkhill. As such, the Cumbria Constabulary Architectural Liaison Officer has been consulted on the proposal. Whilst officially raising no comment, he has verbally advised that there is no actual or anecdotal evidence to suggest that this type of development is associated with an increased risk of criminal activity.
- 5.25 Concern has also been raised over the potential for anti-social behaviour caused by 'partying' tourists. The Cumbria Constabulary Architectural Liaison Officer has verbally commented that whilst this type of anti-social behaviour was historically a problem on the large Lake District camp sites, it has now largely been 'managed out' through, for example, the adoption of policies refusing admittance to large, single sex groups of tourists. It is therefore considered that through good management of the site, anti-social behaviour could be minimised. It must also be remembered that the proposed site is largely seeking to attract walkers, cyclists and visitors to Hadrian's Wall and will offer limited services. It is therefore likely that the site would attract visitors wanting a different experience to the large Lake District sites that retail alcohol and provide on-site entertainment.

8. Impact On The Living Conditions Of Neighbouring Residents

- 5.26 There have been a number of general objections to the proposal on the basis that it will adversely effect the living conditions of residents of Monkhill and

Beaumont through an increase in the number of people walking through both villages. There has also been a specific objection from the resident of Lock Cottage as he considers that the proposal will result in a direct loss of his privacy and amenity.

- 5.27 Policy CP5 of the Carlisle District Local Plan seeks to ensure that there is no adverse effect on residential amenity arising through development. Whilst acknowledging the general objections raised, it should be remembered that the proposed site is relatively small and will cater for a limited number of tourists at any given time. Members should also be aware that the HWPNT already passes through Beaumont and so - given that the site is primarily aiming to attract walkers - it is likely that a high percentage of people using the site would pass through Beaumont regardless of the presence or otherwise of the application proposal. In addition, given that Monkhill is farther from the HWPNT than the site and has little to offer in terms of tourist facilities, it is reasonable to assume that not everyone using the camp site will journey as far as Monkhill. If people require, for example, evening meals, it is more likely that they will continue along the trail to Burgh-by-Sands. It is acknowledged that if the Public House in Monkhill were to re-open, this would act as a draw to people using the site. However, it is still not considered that this would lead to people visiting Monkhill in sufficient numbers to adversely effect the living conditions of residents.

- 5.28 The concerns raised by the resident of Lock Cottage in relation to loss of privacy and amenity are acknowledged. However, any assessment of likely impact must be made on the basis that, at its closest point, Lock Cottage is some 60m from the boundary of the proposed site and a hedgerow, road, agricultural field and bank and a number of trees are located between the two. In addition, as the visitor facilities are located towards the southern end of the site, they would actually be at a distance of some 100m from the house. It should also be noted that the northern end of the site is significantly elevated in relation to the southern end so views of Lock Cottage from the tourist accommodation - and vice versa - would be limited. The key workers accommodation is to be located to the northern end of the site and would be more visible. However, given the distances involved it is not considered that this would have sufficient adverse impact on the living conditions of the resident of Lock Cottage to warrant refusal of the application.

9. Flooding and Drainage

- 5.29 A number of objections have been received on the basis that the road which serves the site is liable to flooding. It has been suggested that this is a result of the poor condition of existing drains in the area and also as the ditches which run along the side of the road are blocked. As this is clearly an existing problem, it needs to be considered whether the development of the site will significantly exasperate the situation. Concern has also been raised as to the condition of the field itself which it has been stated is subject to regular flooding. However, as this element does not affect the public realm, it is not a material planning consideration but rather a matter for the applicant to address should planning permission be granted.

- 5.30 A further letter from the occupier of Lock Cottage has also been received. This questions the occupancy rate on which the initial foul sewerage scheme submitted with the previously withdrawn application was calculated. This is addressed within the revised drainage scheme prepared by Lakeland Environmental Management. He has also stated that two properties within Monkhill discharge into the adjacent storm drains and has provided a letter from the City Council Estates Office from 1997 to confirm this. His concern is that if the field were to flood, users of the site would be exposed to contaminated flood water. The Environment Agency are however aware of these comments and continue to raise no objection to the proposal.
- 5.31 In relation to likely impact on the road, the proposal will introduce little in the way of permanent areas of hard standing. The access drive is to be constructed from gravel and the parking places from reinforced mesh, neither of which will impede surface water drainage. Whilst the timber structures may have some impact, given their footprint in relation to that of the overall site, it is considered that this would be minimal. In relation to foul sewerage, the applicants indicated that this would be dealt with by means of a package treatment plant. Members previously requested that the detail of this scheme be submitted prior to the determination of the application. The applicants have now provided this information. While the calculations are based on an 80% occupancy rate rather than a 100% and a flow of 50 litres rather than the standard 75 litres has been used for tents, even at the higher occupancy levels and flow rates, the amount of waste produced would fall well within the capacity of the proposed system. The proposed sewage treatment and disposal system is therefore considered to be acceptable. The Council Environmental Quality Section - who would be responsible for licencing the site - have confirmed that they have no objection to the proposed system.
- 5.32 In In relation to surface waters, the applicants have now indicated that they intend to implement a water harvesting system with water from the buildings collected and redirected to a holding tank where it will be used to flush the toilets. This would have the benefit of slightly reducing surface water on the site and also provide considerable environmental benefits. While details of the system have not been provided in advance, the scheme could be secured by means of an appropriate condition. The scheme would be constructed in tandem with the proposed foul sewerage scheme.
- 5.33 Notwithstanding the above, even without the provision of the above scheme it is not considered that the proposal would result in sufficient additional surface water reaching the road or adjacent land to warrant refusal of the application. The proposal introduces little in the way of permanent areas of hard-standing and the buildings are set back approximately 8m from the edge of the highway. Without the above water harvesting system, water would be allowed to run directly from the roofs of the buildings on to small gravelled areas around their perimeter where it would soak into the ground in the existing manner. As such, while the provision of the water harvesting system is of benefit, even without it, it is not considered that there would be sufficient grounds to refuse the application on the basis of additional surface water

reaching the road.

10. Whether The Provision Of Key Workers Accommodation Is Acceptable

- 5.34 The report so far has concentrated on the acceptability of providing tourist accommodation. However, the second major aspect of the proposal relates to the provision of key workers accommodation on site. This element of the proposal has raised significant concern largely on the basis that a) the proposal is not of a sufficient size to warrant the provision of the dwelling and b) the tourist element of the proposal is merely a means for the applicants to receive permission for a dwelling in a location in which it would generally be considered unacceptable.
- 5.35 The Good Practice Guide on Planning for Tourism states that for many types of holiday parks a residential managerial presence is often essential to achieve quality service to the customer, security for the property and to meet the obligations of health and safety. In order to assess proposals for staff accommodation the Guide suggests that consideration should be given to annex A in PPS7 which requires the applicant to meet the criteria set out in paragraph 12.
- 5.36 Policy H7 of the local plan provides guidance in respect of dwellings essential to agriculture, forestry and other rural based enterprises. This policy is based on the guidance in PPS7 annex A. It is also relevant to note that the proposed accommodation is in the form of a timber cabin that can be relatively easily removed from the site should the proposed enterprise prove unsuccessful. As such, Members can consider the imposition of a condition that either gives temporary permission for the cabin or requires the cabin to be removed and the land reinstated should the proposed enterprise cease operation. Any further accommodation would require a separate application. The applicants have submitted a business plan that indicates the phased introduction of the camping and self catering cabins over a three year period. As such, it is proposed to attach a condition to any permission that may be granted to ensure the phased introduction of the holiday accommodation actually takes place alongside the provision of the proposed key workers accommodation. A condition is also proposed requiring the removal of all structures and the restoration of the land within a given time scale should the venture fail.

Conclusion

- 5.37 On the basis of the above assessment, the proposal is recommended for approval subject to the imposition of relevant conditions.

6. Human Rights Act 1998

- 6.1 Several provisions of the above Act can have implications in relation to the consideration of planning proposals, the most notable being:

Article 6 bestowing the "Right to a Fair Trial" is applicable to both applicants seeking to develop or use land or property and those whose interests may be affected by such proposals;

Article 7 provides that there shall be "No Punishment Without Law" and may be applicable in respect of enforcement proceedings taken by the Authority to regularise any breach of planning control;

Article 8 recognises the "Right To Respect for Private and Family Life";

- 6.2 **Article 1 of Protocol 1** relates to the "Protection of Property" and bestows the right for the peaceful enjoyment of possessions. This right, however, does not impair the right to enforce the law if this is necessary;

- 6.3 The proposal has been considered against the above Protocol of the Act but in this instance, it is not considered that there is any conflict. If any conflict was to be alleged it is not felt to be of sufficient weight to refuse planning permission.

7. Recommendation - Grant Permission

1. The development shall be begun not later than the expiration of 3 years beginning with the date of the grant of this permission.

Reason: In accordance with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended by Section 51 of the Planning and Compulsory Purchase Act 2004).

2. This permission relates solely to the development of 2 self catering cabins, 3 camping cabins and 10 tent pitches which should be used solely for short term holiday letting for not more than 21 days at any time and following the expiry of such period those persons occupying the cabin / pitch shall not re-occupy any pitch / cabin within 28 days.

Reason: The site is within an area, where to preserve the character of the countryside it is the policy of the local planning authority not to permit additional residential development and to ensure compliance with Policy H1 and EC16 of the Carlisle District Local Plan

3. The key workers accommodation, camping cabins, self catering cabins, service cabin, washing/drying room, storage shed and all associated infrastructure shall be removed from the site, and the ground reinstated to its

SCHEDULE A: Applications with Recommendation

09/0017

former condition, if the facility ceases to operate.

Reason: To ensure that the accommodation is only occupied in association with the operation of the tourist facility and that the development complies with the objectives of Policy H1 and EC16 of the Carlisle District Local Plan (2001-2016).

4. The key workers accommodation, identified on the block plan, shall be occupied only by the site manager and his/her immediate family.

Reason: To ensure that the accommodation is only occupied in association with the operation of the tourist facility and that the development complies with the objectives of Policy H1 and EC16 of the Carlisle District Local Plan (2001 - 2016).

5. The site manager/owner shall keep a register to monitor the occupation of the 2 self catering cabins, 3 camping cabins and 10 tent pitches hereby approved. Any such register shall be available for inspection by the Local Planning Authority at any time when so requested and shall contain details of those persons occupying the holiday caravans, their name, normal permanent address and the period of occupation of the holiday caravan.

Reason: To ensure that the holiday caravans are not occupied as permanent residential accommodation and to ensure that the development complies with Policies H1 and EC16 of the Carlisle District Local Plan (2001 - 2016).

6. Before use of the site hereby permitted approved commences, details of the proposed;

1. Crime prevention and site security measures

2. Noise control measures

Shall be submitted, to and approved in writing by, the Local Planning Authority

Reason: To avoid a negative impact on the living conditions of neighbouring residents

7. Trees and shrubs shall be planted in accordance with a scheme to be agreed with the local planning authority before building work commences and the trees and shrubs shall be retained and maintained to the satisfaction of the local planning authority. The scheme shall include the use of native species and shall also include a detailed survey of any existing trees and shrubs on the site and shall indicate plant species and those trees and shrubs to be retained.

Reason: To ensure that a satisfactory landscaping scheme is prepared in accordance with the objectives of Policy EC16 of the Carlisle

SCHEDULE A: Applications with Recommendation

09/0017

District Local Plan.

8. Other than those trees identified for removal on the approved plan, no tree or hedgerow existing on the site shall be felled, lopped, uprooted or layered without the prior consent in writing of the local planning authority and the protection of all such trees and hedgerows during construction shall be ensured by a detailed scheme to be agreed with the local planning authority.
- Reason:** The local planning authority wishes to see existing hedgerows/trees incorporated into the new development where possible and to ensure compliance with Policy CP3 of the Carlisle District Local Plan.
9. All works comprised in the approved details of landscaping shall be carried out in the first planting and seeding season following [occupation of the building/dwelling or completion of the development,] whichever is the sooner.
- Reason:** To ensure that a satisfactory landscaping scheme is implemented in accord with Policy CP3 and EC16 of the Carlisle District Local Plan.
10. Before the use of the site hereby permitted commences, the whole of the access area bounded by the carriageway edge, entrance gates and the splays shall be constructed and drained in accordance with details submitted to, and approved in writing by, the Local Planning Authority.
- Reason:** In the interests of road safety. To support Local Transport Plan Policies: LD5, LD7, LD8
11. The development shall not commence until visibility splays providing clear visibility of 2.4metres by 70metres measured down the centre of the access road and the nearside channel line of the major road have been provided at the junction of the access road with the county highway. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995 (or any Order revoking and re-enacting that Order) relating to permitted development, no structure, vehicle or object of any kind shall be erected, parked or placed and no trees, bushes or other plants shall be planted or be permitted to grown within the visibility splay which obstruct the visibility splays. The visibility splays shall be constructed before general development of the site commences so that construction traffic is safeguarded.
- Reason:** In the interests of highway safety. To support Local Transport Plan Policies: LD7, LD8
12. The use of the development shall not be commenced until the access has been formed with 4.5 metre radius kerbs, to give a minimum carriageway width of 4.8 metres, and that part of the access road extending 6 metres into the site from the existing highway has been constructed in accordance with

SCHEDULE A: Applications with Recommendation

09/0017

details approved by the Local Planning Authority.

Reason: In the interests of highway safety. To support Local Transport Plan Policies: LD7, LD8

13. Access gates, if provided, shall be hung to open inwards only away from the highway, be recessed no less than 6.5m as measured from the carriageway edge of the adjacent highway and shall incorporate 45 degree splays to each side.

Reason: In the interests of highway safety. To support Local Transport Plan Policies: LD7, LD8

14. The use shall not be commenced until the parking requirements have been constructed in accordance with the approved plan. Any such parking provision shall be retained and be capable of use when the development is completed and shall not be removed or altered without the prior consent of the Local Planning Authority.

Reason: To ensure a minimum standard of parking provision when the development is brought into use. To support Local Transport Plan Policies: LD5, LD7, LD8

15. The use hereby permitted shall not be commenced until drainage works to serve the development have been completed in accordance with the proposed Sewage Treatment and Disposal System recommended in the report prepared by Mr D Clayton of Lakeland Environmental Management and dated 6th April 2009

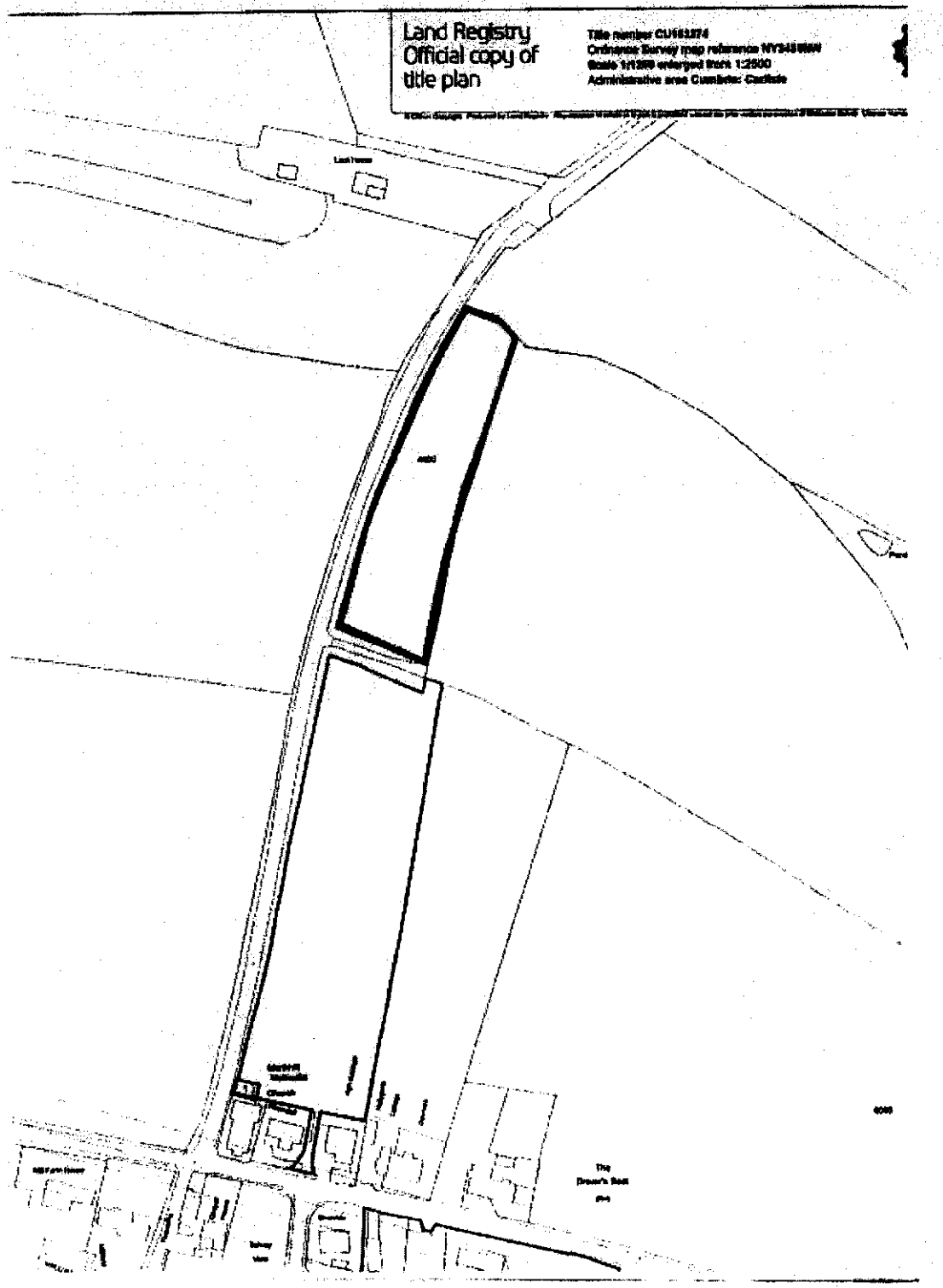
Reason: To ensure that adequate drainage facilities are available.

16. The use hereby permitted shall not be commenced until a scheme for the harvesting of surface water has been approved in writing by the Local Planning Authority. Such a scheme shall be constructed and completed in accordance with the approved plans.

Reason: To ensure the appropriate treatment of surface waters

17. Prior to the commencement of the hereby permitted development on any part of the site there shall be submitted to and approved in writing by the Local Planning Authority, a plan and/or programme showing the proposed phasing of the development and the development shall thereafter proceed only in accordance with the approved phasing plan and/or programme or such variation to that plan and/or programme as may subsequently be agreed in writing by the Local Planning Authority.

Reason: To secure in the public interest a satisfactorily correlated order of development.

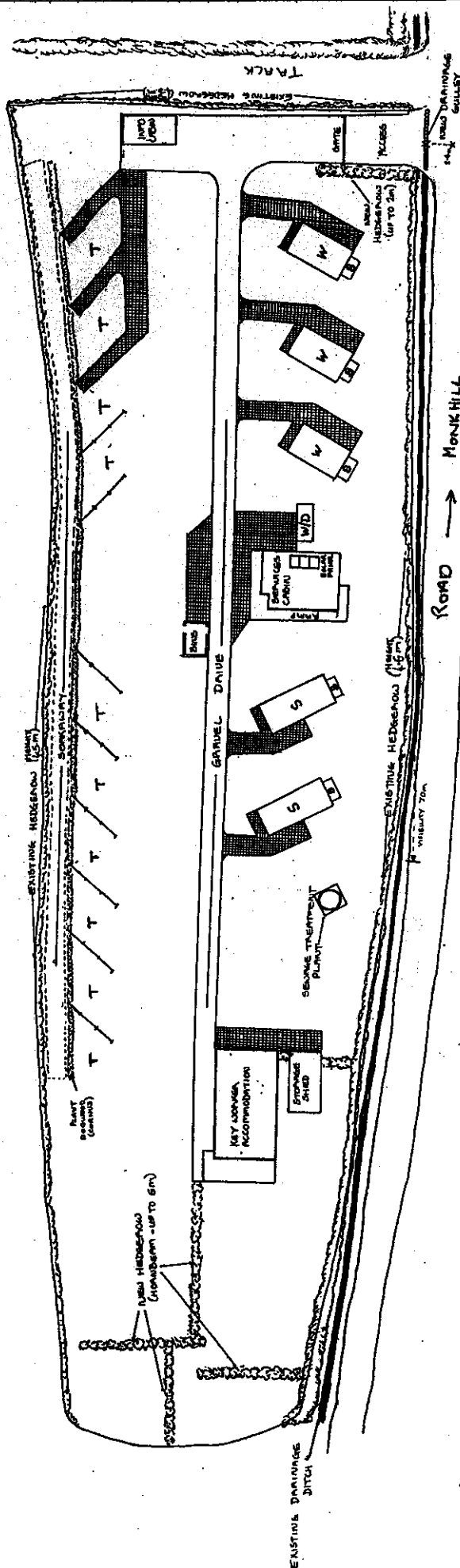


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Roman Wall Lodges - Monk Hill

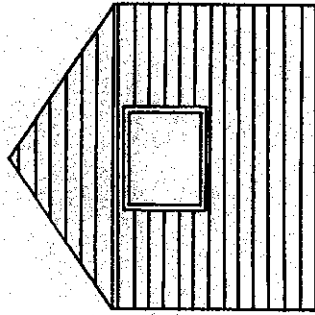
Key	
	Parking Place Grass (retaining wall with grass growing through)
W	Wagon (camping cabin)
S	Shedding (self-servicing cabin)
B	Service Site Storage (+ additional space in storage shed)
Service Cabin	Disabled access to Shedding area & Lounge
W/D	Washing up/drying room
T	Test Platform (supported by additional handles - approx. 2m height)
Site	Public Caravan and Shedding (supported by additional handles - approx. 2m height)



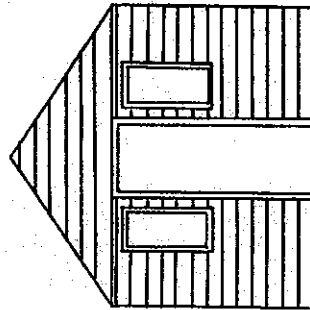
PROPOSED BLOCK PLAN OF CAMPSITE FIELD, MONK HILL FARM, BURNBY SANDS - CAS 60B

SCALE: 1:200

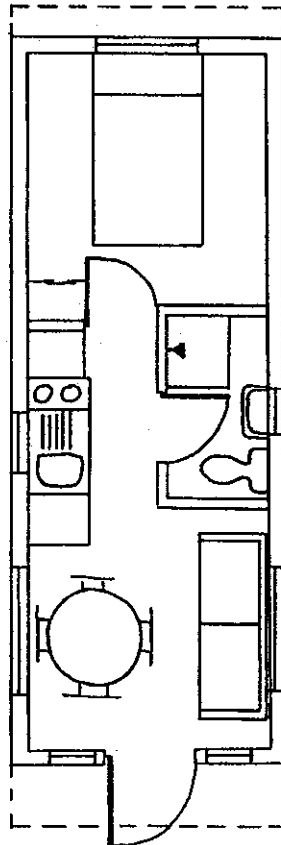
<i>Walter Pelt</i> 13/10/08
SHEILING S/C CABIN
PROPOSED FLOOR PLAN ELEVATION
MR M. DOWERTY
ROMAN WALL LODGES
SCALE: 1/50 11/10/08



REAR ELEVATION

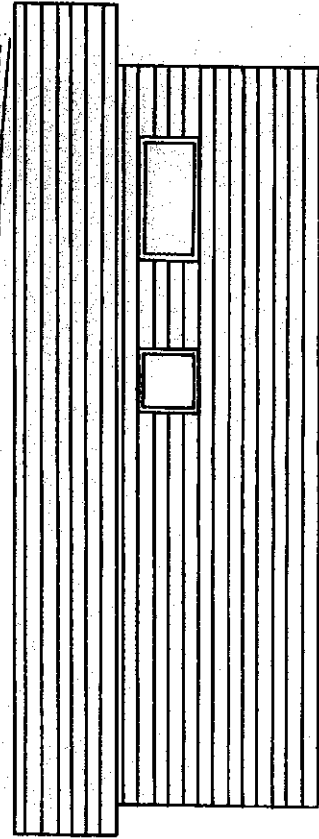


FRONT ELEVATION

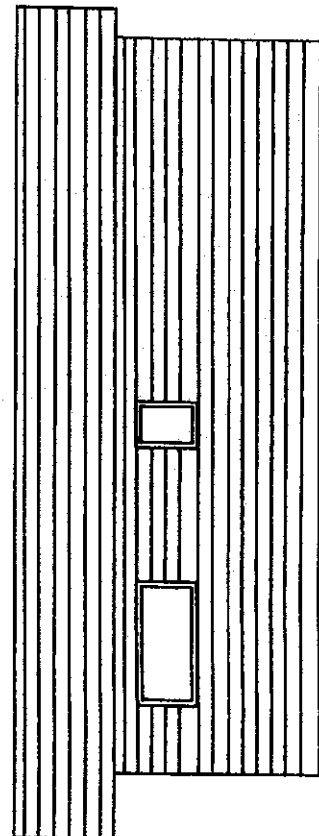


FLOOR PLAN

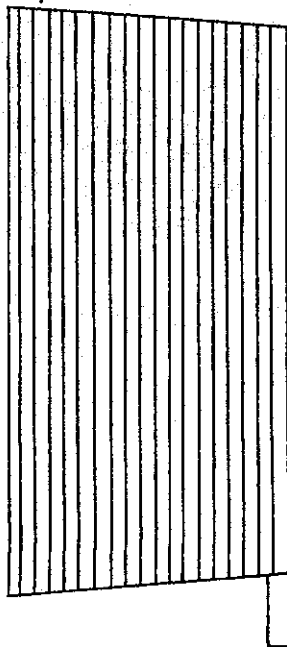
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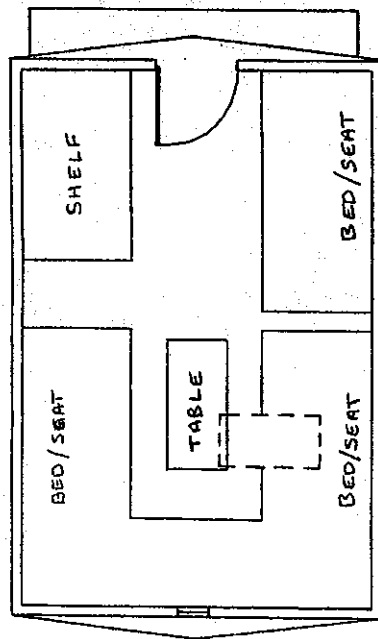
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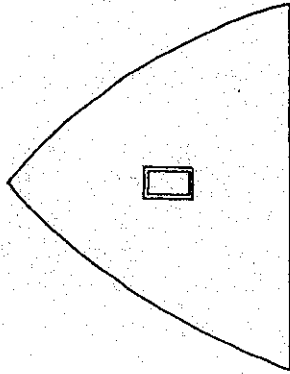
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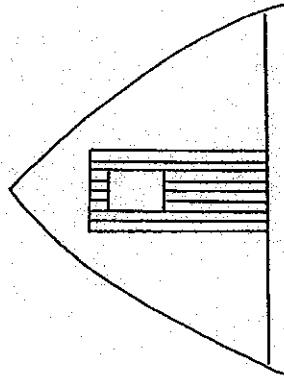
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FLOOR PLAN



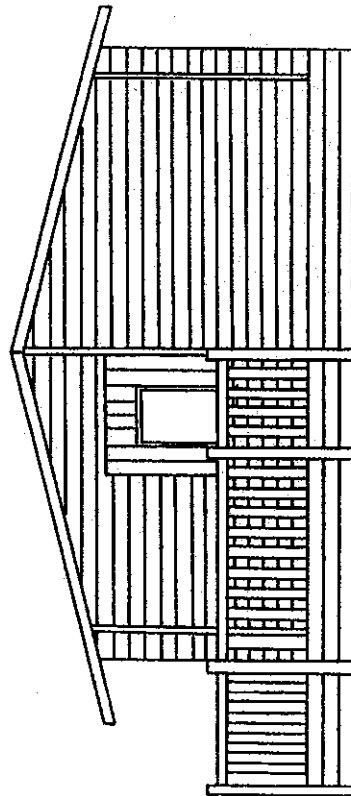
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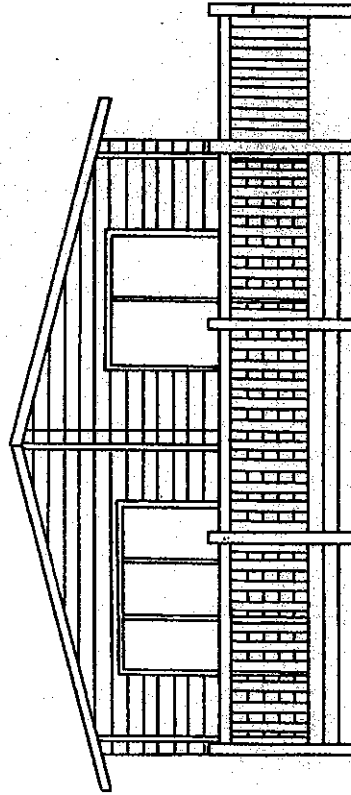
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WIGWAM	CAMPING CABIN
PROPOSED FLOOR PLAN & ELEVATION	
MR. M. DOHERTY	
ROMAN WALL LODGES	
SCALE: 1/50	
11/10/08	
Date Recd 13/10/08	



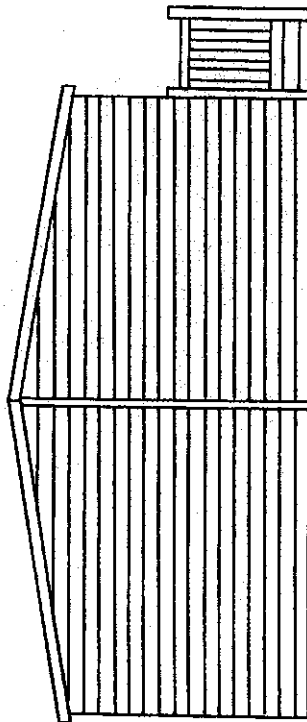
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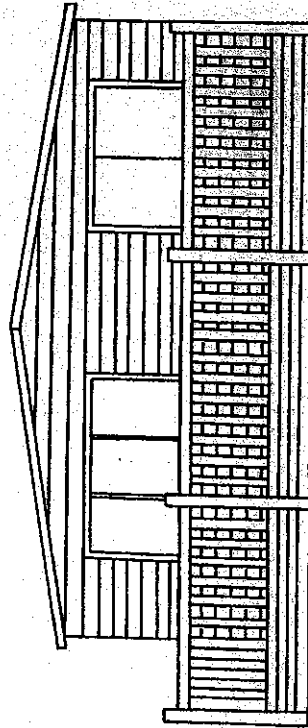
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SERVICE CABIN
PROPOSED ELEVATIONS
MR M DOHERTY
ROMAN WALL LODGES
SCALE: 1/50
<i>John Doherty</i> 13/10/08



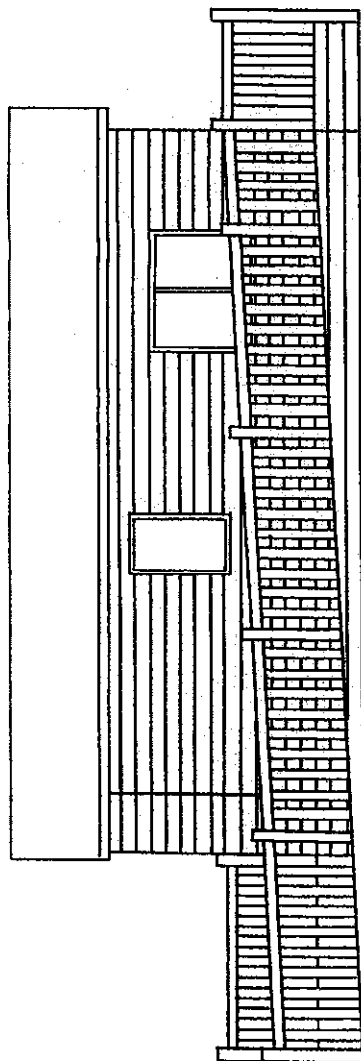
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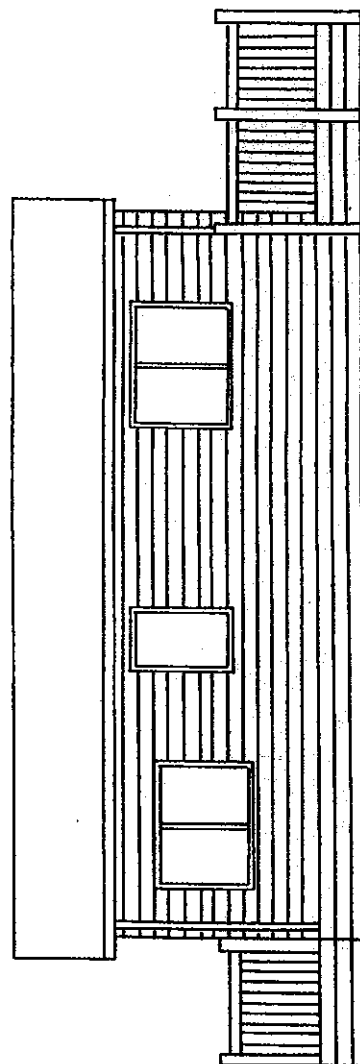
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PROPOSED ELEVATIONS
MR M DOHERTY
ROMAN WALK LODGES
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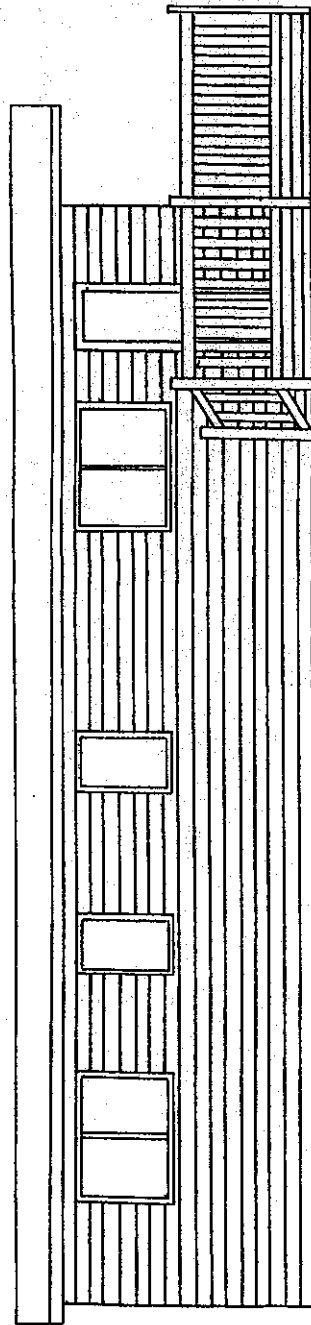
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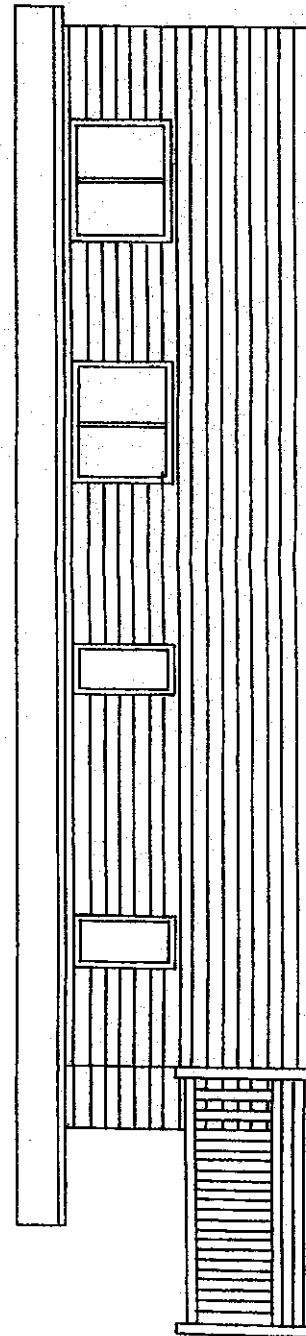
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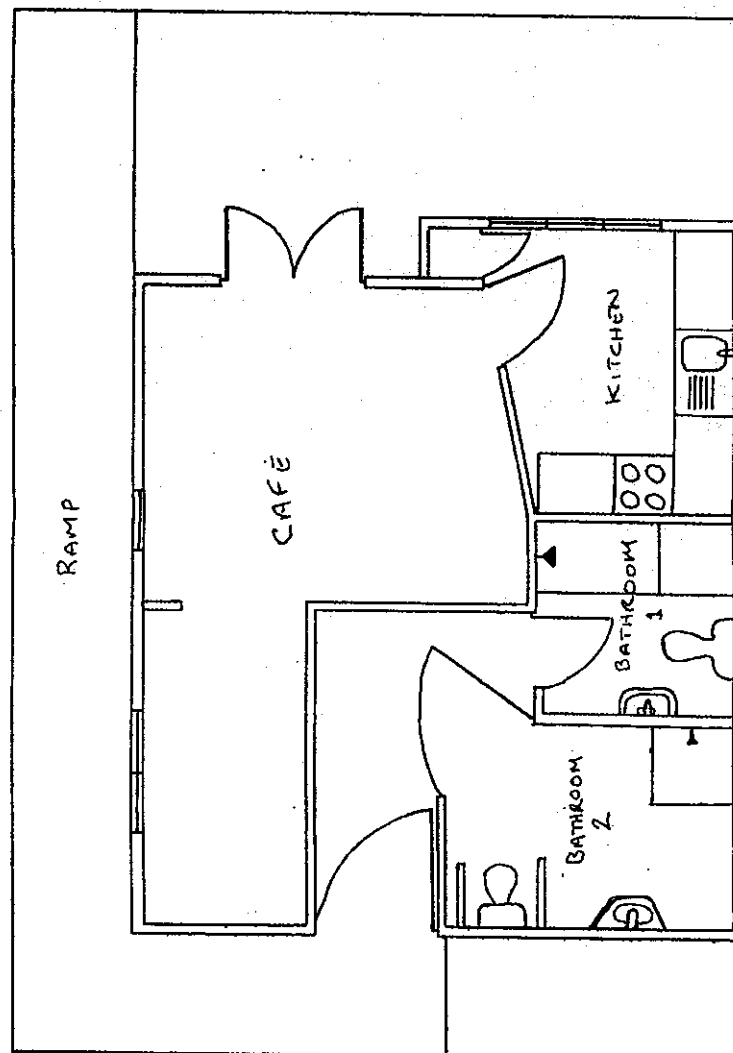
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SIDE ELEVATION

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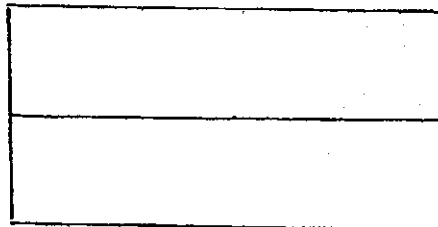
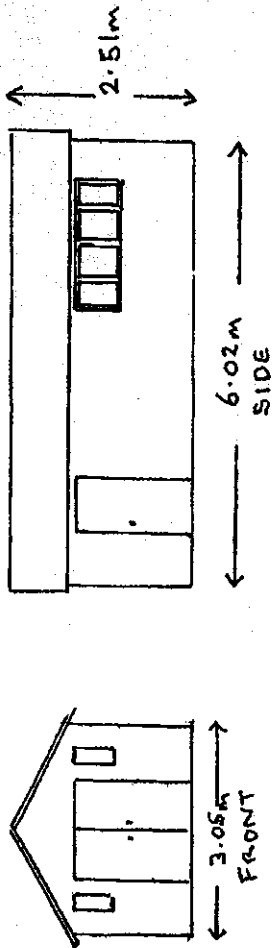
OWNER'S CABIN
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MR M DOHERTY
ROMAN WALL LODGES
SCALE 1/50
<i>Mark Doherty</i> 13/10/08



FLOOR PLAN

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SERVICE CABIN
PROPOSED ELEVATION
MR M DOHERTY
ROMAN WALL LODGES
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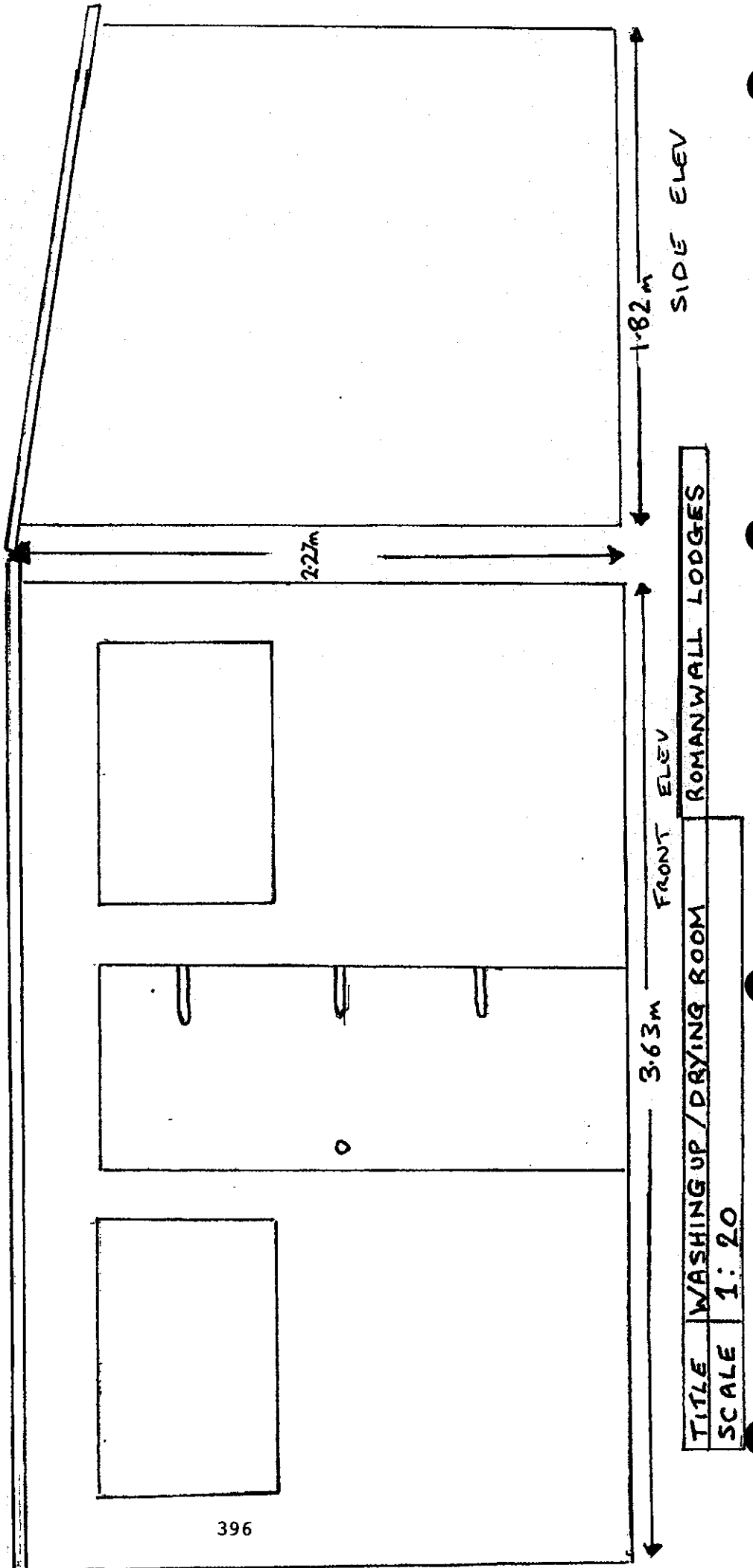
PLAN

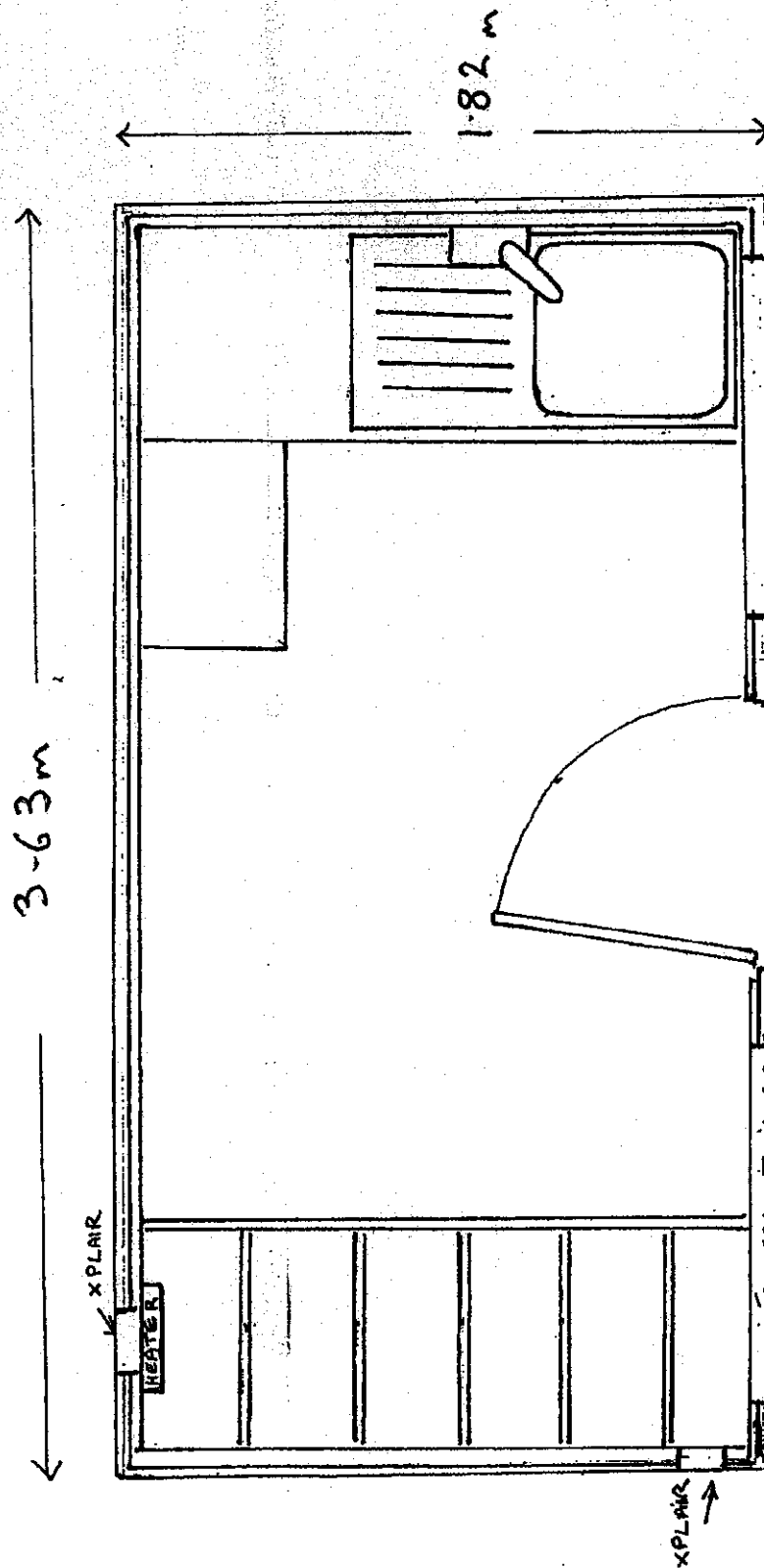
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TITLE	MAINTENANCE SHED / BIKE STORAGE FOR TENT USERS	
SCALE	1 : 100	ROMAN WALL LODGES

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PENT SHED, SEE ATTACHED DOCUMENTS FOR DETAILS





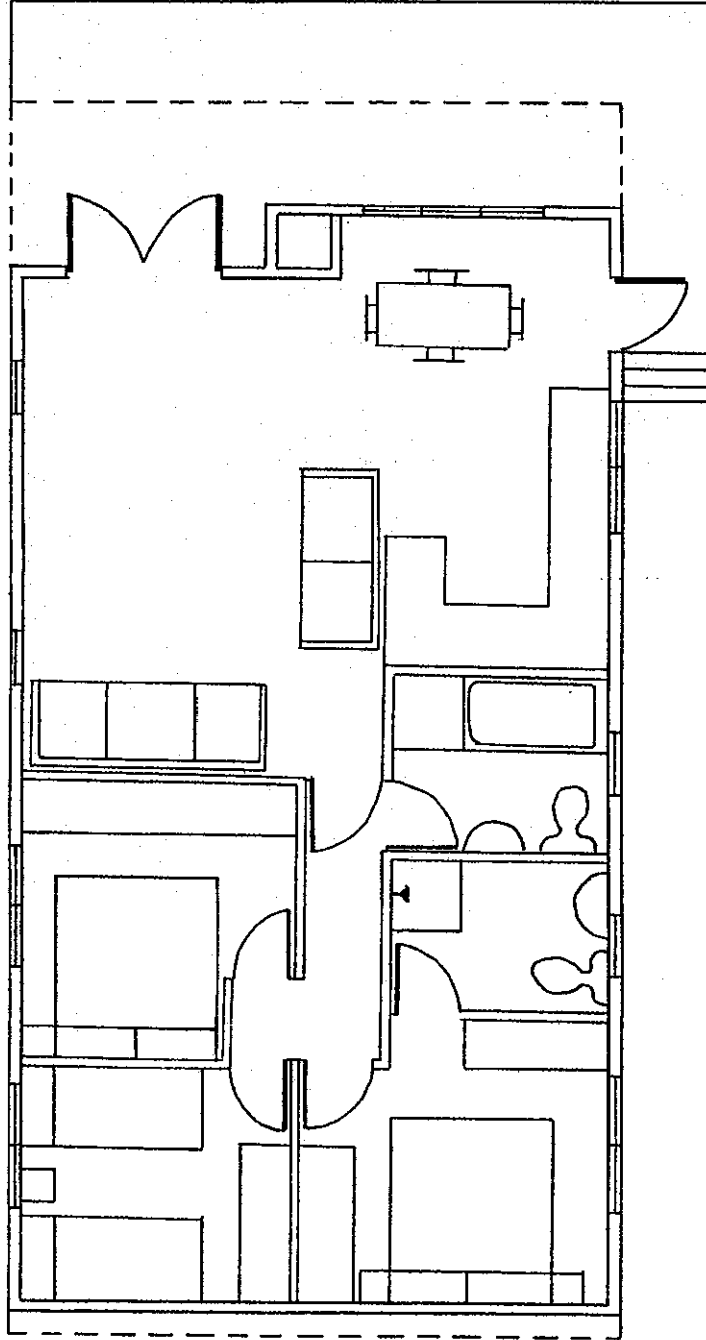
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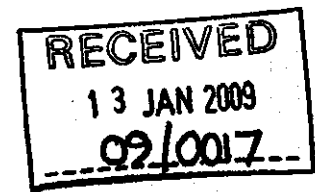
PLANNING & HOUSING SERVICES	
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JAN 2009	

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JAN 2009
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FLOOR PLAN

OWNERS CABIN
PROPOSED FLOOR PLAN
MR M DOHERTY
ROMAN WALL LODGES
SCALE: 1/50
11/10/08
<i>John Doherty</i> 13/10/08



Roman Wall Lodges and Camping Park



Business Plan (September 2008)

Martin Doherty & Julie Ballantyne

Roman Wall Lodges – Business Plan

Summary

We are looking to set up a small camping park with a varied range of accommodation on offer, primarily, to walkers and cyclists using the Hadrian's Wall trail.

Our site is literally only a five minute walk from where the national trail route passes through the village of Beaumont to the west of Carlisle. The site, placed between Monkhill and Beaumont, is in a prime location – only 8 miles (40 minute bus journey) away from the start of the path at Bowness, yet only 3.5 miles (8 minute bus journey) away from the city of Carlisle.

Sited in a small field at the roadside, the site is naturally screened by hedgerow and the proposed development will have virtually no visual impact. This is important given the proximity of the site to the Area of Outstanding Natural Beauty to the north, and the fact that the site itself sits within an area identified as being a World Heritage Site. The small scale of our proposal will provide much needed accommodation for the increasing number of visitors to the area, but will have virtually no impact on the local environment and surroundings. In fact, we would hope that the only local impact is very positive: increased use of local facilities such as the local pub and bus services, food sourced from local farmers wherever possible. Where feasible, we will be using local farmers and contractors to carry out work on site and we also hope to employ local people on site at a later date.

We are both active walkers and therefore, have a vested interest in being able to offer a high quality service to other walkers, cyclists and campers like ourselves. We have accumulated lots of experience, knowledge and skills which we believe can be put to good use to enhance the experience of visitors to our site. We believe we can offer something slightly different and unique which will complement the offer already available along the Hadrian's Wall trail.

We are actively committed to promoting a sustainable business – our aim is to be able to set up and run a business with as little environmental impact as possible. To this end, we will actively look to use as many environment friendly methods and materials in setting up our business.

We have carried out a lot of research both in investigating the 'viability' of our business idea, and also in finding suitable and appropriate methods for carrying out our business. Informal discussions and consultations we have had to date have confirmed that we have a 'sound' business proposition; this document seeks to set out exactly how we intend to go about making our vision a reality

Background

Personal skills relevant to the proposition:

We are both very keen walkers and campers and have lots of experience in this country and abroad. As well as walking and camping as a couple, we also have experience as part of a group. We have stayed in many different kinds of facilities and, more often than not, been disappointed with the standard and quality of facilities and services offered to campers and walkers.

When we were on holiday in America a few years ago, we were both very impressed with the idea of providing camping cabins and quality facilities, which was the "standard" offer in America. We were both at a stage when it was possible for us to consider some "life-changing" ideas and so, the idea of developing a business catering for people, like ourselves, who want to enjoy walking and camping holidays but with a few "comforts" that make the difference between a "pleasant and enjoyable" holiday, as opposed to a camping holiday where you just have to accept the fact that you will be faced with sub-standard washrooms and services, and have access to few, and often poor facilities for the length of your stay.

We are both easy going people who find it easy to mix with others and will have no problem accommodating a whole range of individuals and groups who may want to use our facilities. Between us, we are able to offer the following skills and experience:

- Over 20 years experience of organising varied accommodation for group walking and camping trips throughout Britain
- Experience of organising walking and camping trips for local walking club
- Health and Safety training courses – roles and responsibilities, key manager, risk assessment, stress management
- Experience of working in the hotel and catering industry (bar manager and licensee, waitress, receptionist)
- Practical skills and experience (electrical work, woodwork/carpentry, running own internet and market business, computer skills)
- Over 20 years experience of working in the Public Services Sector
- Experience of hill-walking and mountaineering both in Britain and abroad
- Member of an environmental campaign group

Once we decided to look at the prospect of setting up such a business we then looked for possible opportunities. We visited Hadrian's Wall very shortly after coming back from America and through a conversation we had with the owner of the accommodation where we stayed at that time, who told us how much he was struggling to cope with the additional demand for accommodation due to the increasing number of visitors to the trail route, we decided to look more closely at the possibility of setting up such a business along Hadrian's Wall.

The market and its potential¹:

Our main aim is to cater for walkers and cyclists using the Hadrian's Wall trail. Statistics and available data tell us that most people on Hadrian's Wall tend to walk as couples (63% in 2007). Most people prefer to stay in B&B accommodation (42%), with a smaller number planning to camp (18%). Our aim is to provide quality accommodation at a reasonable price to these groups, which accounted for some 60% of those people walking the Hadrian's Wall trail in 2007. We will be targeting cyclists as well as walkers, as this "market" has not yet been fully realised in forecasts of future visitor numbers.

We know that we are going to offer something different to what is already available elsewhere. Our accommodation will cater for those people who want a quality, reasonably priced alternative to standard B&B accommodation, or those who aren't willing to "rough it" in a bunkhouse or use the usual type of campsite on offer. The cabins that we are intending to use have been designed by an award-winning Scottish architect who has gained some recognition for his work in developing such buildings. Once we put these cabins on site we will also be linked to the "Wigwam Holidays" organisation, which through a successful website, has become very popular with families looking for a slightly "different" type of holiday. On speaking to other "Wigwam" site owners, we have been told that bookings are generally very good, so this would be an additional "market" for us in terms of business viability.

Key to our success will be our ability to market ourselves correctly to ensure that we get information and details about our service offer to the right people. Again, we have consulted closely with other experts about this and, as much as possible, we will be making use of existing websites, brochures and leaflets etc.

As well as attracting people using the Hadrian's Wall trail, we are also aware that our location is ideal for people to use as a touring base – within quick and easy access to the Lake District, the Solway coast (particularly popular with birdwatchers) and Scotland. It's proximity to Carlisle, with its excellent transport links, could make it an attractive place to stay for those seeking the conveniences of "city life" without the hassle. A place which can offer the attractions and facilities of city life, but where people can escape from the hustle and bustle.

Although we expect most of our visitors to be seasonal (April – October), we are also hoping to attract a small number of visitors at other times for the reasons mentioned above. We have taken a pro-active decision to not cater for caravans and tourers as there are other facilities in the area and also, we feel that additional road traffic would affect quality of life in this area. The data indicates that very few people choose to do the trail using a caravan. In our experience, people in caravans are also looking for slightly different facilities.

¹ All statistics and data have been taken from 'Hadrian's Wall Path National Trail: Economic Impact Study and Trail User Analysis 2003 – 2007'

Our market will always be limited, in terms of growth, by the size of our site. We do not want to compromise quality by becoming overcrowded and "under-serviced". We believe our finance plans indicate that a business on a small scale can still be viable for us. Initially, we will place a smaller number of cabins on site until we can know which type of accommodation is most popular.

Strategic drivers:

At the time we decided to actively look at the possibility of setting up our site along the Hadrian's Wall trail, the Bowness – Carlisle section of the trail had only recently been opened up, and so we decided to focus our attention in this area. Initial desktop research indicated that there was a distinct lack of facilities or services in this area. This was confirmed by several visits to the area. Aside from a small number of B&Bs, bunk houses and small campsites there is very little accommodation available outside the city to the west of Carlisle.

Since the path opened there has been a steady increase in the number of walkers using the Hadrian's Wall trail. There has also recently been a coast-to-coast cycle route opened up following the wall. This has resulted in quite a dramatic increase in the numbers of people visiting Hadrian's Wall. The numbers increased by 11% in 2007 and figures look set to increase again and in following years as a result of increased publicity and greater "public awareness", promoted by recent TV programmes and the Hadrian Exhibition at the British Museum.

Our discussions and initial queries with officials and experts have generally been met very favourably to date. The scale of our proposed business is in line with regional and local planning guidance. Because of the sensitive location of our site – i.e. close to the AONB and part of the World Heritage Site, we have been very careful to consult and liaise with all available experts to ensure that our site has minimum impact on the historical, archaeological and environmental aspects. This process of consultation, spanning some considerable time includes the following:

- Hadrian's Wall Heritage LTD – help and advice from Julie Sloan and Marie Whitehead, Development Manager, West
- Carlisle City Council, Planning – pre-planning help and advice from Colin Godfrey
- Carlisle City Council, Environmental Health –help and advice from Janet Blair
- Carlisle City Council, Building Control – help and advice from Mark Bowman
- Carlisle City Council, Cumbrian Highways – help and advice from Pieter Barnard
- Carlisle City council, Economic Development – help and advice from David Beatty, Head of Economic Development
- Carlisle City Council – help and advice from Don Taylor, Business Development Manager

- Cumbria Rural Enterprise Agency (CREA) – help and advice from Gemma Barnes and Len Cockroft
- English Heritage – help and advice from Mike Collins, Archaeologist
- County Archaeologist's Office – help and advice from Jeremy Parsons, County Archaeologist
- Business Link North West

As well as consulting with the above organisations we have also referred to several policies and documents which also support our proposal. These are listed in an appendix to this Plan.²

Wherever possible, given scale and financial constraints, we will be trying to use sustainable and environment friendly materials and energy saving equipment and procedures:

- The driveway and "on-site" tracks will be made from recyclable materials which have low visual impact
- We will be using the latest sewage treatment technology and soakaway system in consultation with the Environment Agency
- We intend to use solar water heating panels at the earliest opportunity to supply hot water to the services cabin
- We intend to use lower power level lighting systems throughout the site, taking into consideration any health and safety issues
- We will work with United Utilities to find the most efficient methods of water and electricity conservation
- We will also be working within guidelines for energy conservation from the Government body; Energywatch
- We intend not to increase the amount of "run-off" – all drives and ground area will have the same existing ability to provide soakaway and drainage into the natural ditch running alongside the site, which we will actively maintain
- The cabins themselves are designed for the rural environment and utilise sustainable resources of home grown timber in construction. This gives them a very low carbon footprint
- We will be providing collection points for the purpose of recycling materials such as cans, bottles, cardboard and plastics
- We will be planting native species of trees and bushes e.g. hornbeam, dog rose and other species
- 80 % of the site is surrounded by hedgerow and cabins and tents will not be visible from any properties or the roadway
- The remaining 20% of the site will be screened using a native hornbeam hedge
- All visitors to the site will be expected to adhere to site policies around noise, disturbance and any other impact such a site may have on the surrounding area – to this end we will not be taking bookings from large groups and the size of the site will keep disruption to a minimum

² Please refer to Appendix 1: References

Customers' priorities and needs:

Most people looking for accommodation when carrying out a venture as walking/cycling the Hadrian's Wall route are looking for a range of facilities and services. The list below is not exhaustive but highlights services and facilities we believe are important to walkers and cyclists and which, we would aim to offer to visitors to our site:

- Open access – i.e. people can come and go at times to suit them
- Warm and comfortable accommodation that isn't overpriced or offers lots of "extras" which they won't have time to make use of
- Reasonably priced accommodation – we know from available data that most people will expect to pay between £20 - £40 per night
- Access to "trivial" items e.g. spare socks, plasters, puncture repair kits,
- Food and drinks – at times to suit them i.e. early/late breakfasts
- Packed lunches made to order using good quality 'healthy' ingredients
- Flask top-up
- Ability to pre-order food and other items in advance of visit – especially for guests staying in self-catering accommodation
- Option to cook outdoors
- Secure storage for cycles
- Cycle wash facility
- Drying room for boots and clothes
- Local pub offering evening meals and good facilities such as pool, TV, darts etc.
- Games/lounge/meeting area
- Proximity to the trail – people do not want to spend unnecessary time trekking to accommodation from the trail
- Friendly atmosphere
- Maps and information about the local area and the Hadrian's Wall trail
- First Aid and access to emergency health services

We believe our site will meet all of the above criteria and we will therefore be able to offer a quality service to our customers. Once our business has started up we will be looking to be assessed and gain a rating from the VisitBritain organisation. We will also be working to meet the standards of the 'Cyclists' Welcome and Walkers' Welcome awards

Our service:

We are looking to provide a small site with a variety of accommodation on offer to meet a range of needs (and budget). Our accommodation³ will include:

- The **Shieling** – fully self-catering with fully equipped kitchen and shower/wc. This will comfortably accommodate up to 4 people (2 in the separate bedroom with twin/double beds) and 2 in the lounge on pull-out sofa-beds. The shieling is fully heated and also has TV

³ Appendix 2: Pictures of cabins

- **The Wigwam** – a small camping cabin that can accommodate up to 4 adults (sleeping on a "shelf" -similar to mountain hut accommodation but with comfortable mattresses!) or a family, or 2 people in a twin/double bed formation. The wigwams are equipped with a microwave/grill, kettle and toaster. The wigwam is also fully heated with a TV but has no shower/wc
- **'Ready-tents'** – ready assembled tents sleeping up to 4 adults or a family. The tents will have beds made up and will have storage facilities

As well as the above accommodation we will also have pitches available for up to a total of 10 tents. The pitches are set up to give privacy and to allow camping on 'ready-made' pitches – i.e. flat, levelled ground with stones pre-removed!

We will also have a "Services Cabin" where people can enjoy a cooked, or continental breakfast – if they choose. This will be charged separate to accommodation so as not to penalise those people wishing to make their own arrangements. We will also offer a 'packed lunch' service. We will be using locally sourced and fair trade food items wherever possible. Outside of meal times, the Services cabin will be open for people to meet in/ play games/ watch TV. It is not our intention to offer cooked lunches or evening meals as we would prefer to encourage visitors to use other local facilities – there is a pub located within 50 metres of our site and we feel it is important that we do everything possible to support other local businesses. It is not our intention to duplicate services but to offer something "in addition" to that which is already available.

During daytime hours the Services Cabin will be available for walkers/cyclists to have a coffee/tea and make use of the toilet facilities.

We are also hoping to have a small information kiosk on site where people can read and learn the history of the area throughout the ages. We'd also like people to be able to access information about places of interest so they can make the most of their time spent in the area.

Strategic action and finance plan:

We have allowed almost one year in order to set up the business – this will include time for site preparation, planning permission etc. Please see timeplan attached as Appendix 3⁴. Initially, we will be setting up with 1 Shieling and 2 Wigwams – our intention is to increase these numbers depending on which type of accommodation proves most popular. We have submitted planning permission on the basis of increasing accommodation to a total of 2 Shielings and 3 Wigwams.

Martin Doherty will be the Key Worker and Manager on site and will have responsibility for all matters relating to the site – cooking, cleaning, customer care etc. Julie Ballantyne will continue in her current employment within Adult Education. Julie's work is largely based "working from home" and this will provide

⁴ Appendix 3: Roman Wall Lodges Timeplan

a degree of flexibility to help cope with busy periods. This also provides an additional income aside from the income generated by the business.

Once the business realises its potential then we intend to employ someone local to help Martin with daily chores etc. We anticipate that this will happen at some point in Year 2 of operation depending on numbers.

Our financial planning⁵ is costed on a seasonal basis: a 7 month 'High Season' from April to October inclusive, and a 5 month 'Low Season' November to April inclusive.

Year 1 to year 3 will be the time allocated for the business to reach 80% occupancy during High Season, with an estimated occupancy rate of 60% in year 1, increasing by 10% in subsequent years. We assume 80% occupancy will be the 'norm' once the business is established. We have costed on the basis of 20% occupancy during the Low Season.

Our financial plans have been submitted to Bank Business Managers and Accountants and have received favourable comment. We have estimated that it will take approximately 6 to 7 years for the business to become fully viable and self-sustainable. Although we have tried to cost as accurately as possible, figures will obviously need to be revised once an opening date has been established.

Now that we have been lucky enough to secure the right size of land in the right geographical area, we feel confident that this business is a worthwhile venture for us, for those living and working in the area and also and for visitors to the Hadrian's Wall trail.

⁵ Please see Appendix 4: Roman Wall Lodges: Setting Up Costs
Appendix 5: Roman Wall Lodges: Budget and Income Forecast

Roman Wall Lodges – Business Plan

References

Allerdale Council: Allerdale Campground Rules

Carlisle City Council. Carlisle District Local Plan 2001 - 2016

Carlisle City Council. Pre-planning advice

Cumbria Rural Enterprise Agency (CREA): Rural Planning Facilitation Service. Planning Assessment Report

Cumbria Tourism – Welcome to Excellence

Frontiers of the Roman Empire WHS – Hadrian's Wall: A New Management Plan 2008 – 13. A Discussions Paper on the Issues on Hadrian's Wall

Hadrian's Wall Path Briefing Notes (period to end December 2005)

Hadrian's Wall Path National Trail: Economic Impact Study and Trail User Analysis, (2008)

Regional Planning Guidance for the North West:

- Policy ER2 Landscape Character
- Policy RU1 Sustainable Agriculture
- Policy RU2 Diversification of the Rural Economy

The Cumbria and Lake District Structure Plan 2001 - 2016

The Department for Communities and Local Government Good Practice Guide on Planning for Tourism 2006: Planning Policy Statement 7: Sustainable Development in Rural Areas

Websites:

www.cyclistswelcome.org.uk

www.hadrians-wall.org.uk

www.nationaltrail.co.uk/hadrianswall

www.nationalcyclenetwork.org.uk

www.cycle-routes.org/hadrianscycleway

Appendix 3

Roman Wall Lodges – Timeplan

Task	Month											
	1	2	3	4	5	6	7	8	9	10	11	12
Purchase of Land												
Planning Application												
Site Preparation												
Installation of cabins												
Landscaping												
Setting up services												
Marketing												
Campsite open												
Key												
Ongoing work												
Fixed period work												
	Year 1				Year 2				Year 3			
Business Review	X				X				X			
Employ staff					X				X			
Additional cabin/wigwam									X			

Roman Wall Lodges - Budget Forecast⁶

Budget Forecast	Year 1	Year 2	Year 3
Business running costs			
Food supplies	<i>3,600</i>	<i>4,000</i>	<i>4,500</i>
Laundry	<i>2,000</i>	<i>3,000</i>	<i>4,000</i>
Cleaning Materials	<i>520</i>	<i>550</i>	<i>600</i>
Extras (food and materials)	<i>1,000</i>	<i>1,000</i>	<i>1,000</i>
Rates	<i>3,000</i>	<i>3,500</i>	<i>3,650</i>
Water	<i>3,000</i>	<i>3,500</i>	<i>3,650</i>
Electricity	<i>3,000</i>	<i>3,500</i>	<i>3,650</i>
Phone and Broadband	<i>480</i>	<i>500</i>	<i>550</i>
Accountant	<i>500</i>	<i>515</i>	<i>530</i>
Insurance	<i>1,000</i>	<i>1,000</i>	<i>1,000</i>
Transport	<i>500</i>	<i>500</i>	<i>500</i>
Advertising	<i>500</i>	<i>500</i>	<i>500</i>
Mortgage payments ⁷	<i>8,400</i>	<i>8,400</i>	<i>8,400</i>
Manager' salary	<i>6,000</i>	<i>6,180</i>	<i>6,365</i>
Additional staff/services		<i>5,000</i>	<i>5,150</i>
Additional cabin/wigwam			<i>35,000</i>
Miscellaneous (TV licence @ £135.50)	<i>1000</i>	<i>1000</i>	<i>1000</i>
Total	32,500	42,645	80,045

N.b. Figures in italics are best guesstimates i.e. based on research but imprecise until definite costing given when business sets up

⁶ As at 22nd June 2008

⁷ Mortgage payments for £75k borrowed to fund setting up of business – approximate monthly payments

Appendix 5 contd.

Roman Wall Lodges - Profit Forecast

Profit forecast	Year 1	Year 2 ⁸	Year 3
Forecast income - high season⁹			
1 Cabins ¹⁰ @ £50 per night in year 1	6,300	7,570	17,808 ¹¹
2 Wigwam @ £30 per night in year 1	7,560	9,114	10,752
10 Tents @ £10 per night	12,600	14,700	16,800
Sales of breakfasts ¹²	8,505	8,757	9,009
Sales of packed lunches	4,410	4,537	4,662
Forecast income - low season¹³			
1 Cabins @ £50 per night	1,550	2,009	2,438
2 Wigwam @ £30 per night	1,860	2,418	2,944
Sales of breakfasts	992	1,285	1,559
Sales of packed lunches	543	702	851
Additional income			
Rent from house to cover mortgage ¹⁴	5,400	5,562	5,729
Carry over from previous year		16,290	30,299
Total Income	48,790	72,944	102,857
Total expenditure	32,500	42,645	80,045
Income - Expenditure	16,290	30,299	22,806

⁸ Year on year income increases based on approx. 3% price increase each year (e.g. £50 - £51.50/£53 year 2/3 for cabins and £30 - £31/£32 year 2/3 for wigwams) in high season and £40 - £41.50/£43; £20 - £21/£22 in low season

Also, assumed increased occupancy from 60% to 70%/80% in year 2/3)

⁹ High Season = April - October = 210 days / Year 1 figures based on 60% occupancy (126 days)

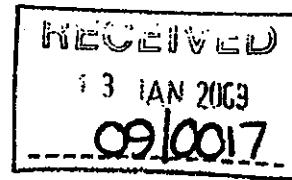
¹⁰ Cabins and wigwams based on 2 people sharing

¹¹ Additional cabin purchased in year 3

¹² Breakfast income based on 10 cooked breakfasts @£5 and 5 continental & £3.50 per day during High Season. Projected price increase of 3% each year plus increased occupancy as per above

¹³ Breakfast income based on 5 cooked @£5 and 2 continental @ £3.50 per day during Low Season. 3% increase year on year. Low Season = November - March = 155 days / Figures based on 20% occupancy (31 days)

¹⁴ Based on income of £450 per month excluding letting agents fees etc.



Supporting Planning Statement

Proposal : Change of use of agricultural land to holiday accommodation comprising of 3no. self-catering units, 10no. tent pitches, access road, alterations to existing vehicular access and construction of 1no. dwelling for occupation by site manager

Location: Land at Field No 4490, Monkhill, Carlisle

Applicant: Mr M Doherty

Contents

1. Introduction and Background
 2. The Site and Its Surroundings
 3. Planning Policy
 4. The Proposal
 - Sustainability
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 - Highway Safety and Accessibility
 - Biodiversity and Drainage
 - The Tourism Industry – Rural diversification and Economic Development
 - Archaeology
 5. Conclusion
- Appendices
- Photographs

1.0 Introduction and Background

- 1.1 This statement is submitted in support of a planning application for the grant of full planning permission for the use of a discrete 1.1 acre piece of enclosed agricultural land as holiday development at Field No 4490, Monkhill, Carlisle, as submitted by Mr M Doherty.
- 1.2 The purpose of this statement is to present the land use planning circumstances, acknowledge relevant planning policy and guidance, describe the impetus and justification for the proposal as a rural economic enterprise and provide an assessment of acceptability against a range of relevant material planning considerations.
- 1.3 Prior to submission, the applicant undertook preliminary discussions and sought advice through the Cumbria Rural Enterprise Agency's 'Rural Planning Facilitation' service and with Mr Colin Godfrey, Assistant Development Control Officer, Carlisle City Council. The response was positive to pursue an application for the development of the site for tourist accommodation.
- 1.4 Discussions have also been held with:

Mr Charles Bennett, Landscape Officer, Carlisle City Council
Mr Pieter Barnard, Development Control Engineer, Cumbria Highways
Mr Jeremy Parsons, County Archaeologist, Cumbria County Council
Mr Gerry Martin,
Percolation Tests
United Utilities
- 1.5 As described below, encouragement has been given from the aforementioned agencies/ persons, which has fortified the our resolve and commitment to providing a high quality, sensitive development, as well as our motivation to develop, strengthen and enhance the proposed rural facility.

2.0 The Site and Its Surroundings

- 2.1 The proposal seeks to change the use of this agricultural land by providing a short-term holiday chalet complex, comprising of 2no. timber cabins, provision for 10no. tent pitches and a service building providing catering facilities and wash facilities. The application also seeks consent for the siting of a timber cabin for occupation by the applicant. Access will be via the existing Monkhill to Beaumont road. It is expected that the Local Planning Authority will attach suitable conditions to any approval, specifying the terms of occupancy and prohibiting the use of the holiday cabins for permanent occupation as 'dwellings'.
- 2.2 An existing vehicular access exists in the south-west corner of the site. This access serves the existing agricultural use of the land and it is proposed that this access be widened and upgraded as part of the development.
- 2.3 The site layout, as described in detail on the submitted drawings, would occupy relatively flat land, that rises towards the northern boundary. The land is framed by existing established and mature hedgerows that is bounded by open countryside. The layout of cabins, tent pitches and parking takes account of the physical constraints of the site together with the archaeological restrictions imposed on the site. Additional landscaping is proposed along the northern boundary of the site.
- 2.4 The site layout plan includes a landscape schedule prepared following advice received from Mr Charles Bennett together with a landscape impact summary statement.
- 2.5 In addition to the site proper, further planting is to be provided along the northern boundary in order to further enhance this natural wildlife setting as well as to soften (glimpsed) views of the site from the north.

3.0 Planning Policy

3.1 Ministerial Advice

3.1.1 PPS7

3.1.2 Most relevant advice on rural development and diversification, including tourism development, is provided within PPS7 "Sustainable Development in Rural Areas". Particular attention is given to paragraph

3.1.3 Later, under Tourism accommodation, Local Planning Authorities are advised to consider the issues of landscape and environmental impact and flood risk, as well as seek to ensure that new sites are not prominent in the landscape and that any visual intrusion is minimised by effective, high quality screening.

3.2 Good Practice Guide On Planning For Tourism

3.2.1 In consideration of this proposal it is recognised that a balance has to be struck between the various planning issues, a valid point that is amplified in the Government's guidance. In essence, three main aspects are related to tourism development, namely: where the development is located; how it is designed; and how the development fits into its surrounds. It is stated in paragraph 5.2 that:

"In order to be commercially competitive and thus successful, tourism developers will choose sites that are accessible to visitors and design their developments in such a way that visitors can readily and conveniently enjoy the attraction or facility."

3.2.2 It is further stated that there will be occasions where tourism related development is sought in a location that will be difficult to achieve the objectives of accessibility to sustainable modes of transport. The choice of location, for example, having been chosen for functional need. Fundamentally, however, planners have to weigh up the benefits from the tourism development against any disadvantages arising from its location.

3.3 Cumbria and Lake District Joint Structure Plan 2001-2016

3.3.1 **Policy ST1:** The promotion of sustainable development, whereby rural communities should be sustained through measures that further the aims and objectives of sustainability, and achieve, inter alia, a flourishing and diverse local economy, quality built, natural and historic environments and vibrant, harmonious and inclusive communities;

3.3.2 **Policy ST3:** The promotion of sustainability principles in all new development through the application of the sequential test on a range of issues and then achieving high standards of design whilst seeking to minimise environmental cost;

- 3.3.3 **Policy ST7:** Support for small scale development in local service centres to help sustain local services, meet local needs or support rural businesses. Outside of local service settlements, exceptions may be allowed where, inter alia, it is needed to sustain existing businesses;
- 3.3.4 **Policy ST9:** Offer priority issues facing North Cumbria, including the need to sustain rural communities by promoting local service centres as a focus for new development of an appropriate scale. Paragraph 2.27 states that whilst development opportunities will be focused in Carlisle, "...it is important that the surrounding rural area is sustained through measures that support and diversify the rural economy";
- 3.3.5 Under "Supporting the Economy" it states in Paragraph 3.4; "The future emphasis will be placed on developing ...sustaining the county's traditional economic activities and promoting opportunities for diversifying in rural areas." Later, under Paragraphs 3.17-3.19 (The Economy in Rural Area), encouragement is given to supporting the rural economy. It states under 3.19; "Whilst Key and Local Service Centres will be the preferred locations for new development, there is a need to strengthen the economy in rural area and provide diversity in the local economic base";
- 3.3.6 Under "Tourism", the importance of such to the Cumbrian economy is re-enforced. Tourism should be developed in a sustainable manner. Reference is made to Policy EM16 (Tourism), with particular reference to supporting text under paragraph 3.25, where it states; *"In rural areas, tourism uses can assist in the diversification of existing businesses and encourage rural regeneration. They will be supported where their scale can be accommodated without adverse effects on the character and amenity of the local area."*
- 3.3.7 **Policy EM16:** Support is given to tourism facilities that enable the economic benefit to the area and for proposals that promote Cumbria's distinctive environmental, cultural and historic character and visitors enjoyment and understanding of it.
- 3.3.8 **Policy E37:** Assesses proposals in respect of their impact in relation to key landscape values in terms of, inter alia, appearance, character, ambience and biodiversity features.
- 3.4 **Carlisle District Local Plan Revised Redeposit Draft 2001-2016**
- 3.4.1 **Policy DP1:** Monkhill is listed as sustainable development location;
- 3.4.2 **Policy CP1:** This affords protection to landscape character and the biodiversity resource against key principles;

- 3.4.3 **Policy CP4:** Considers the likely impact of new development against design principles in the interests of furthering the aims and objectives of sustainability;
- 3.4.4 **Policy CP8:** Encouragement is given to the consideration and use of energy conservation and efficiency in the design of proposals;
- 3.4.5 **Policy CP10:** Affords consideration of the impact of new development upon groundwater and surface water quality conditions;
- 3.4.6 **Policy CP16:** Considers the potential for crime and requires that development contributes to creating a safe and secure environment and minimising the opportunity for crime'
- 3.4.7 **Policy EC11:** Encouragement to proposals to diversify and expand upon the range of economic activities in rural areas;
- 3.4.8 **Policy EC12:** Encouragement to proposals which may assist in the retention of village services;
- 3.4.9 **Policy EC15:** Considers tourism development generally and adds a preference for focusing such upon established settlements or groups of buildings whilst safeguarding the distinctive environment, culture and history of the area;
- 3.4.10 **Policy LE7:** Requires that development within the Hadrian's Wall Buffer Zone do not have an adverse impact on the character or setting of the World Heritage Site;
- 3.4.11 **Policy LE30:** Encourages the reuse of land subject to the satisfactory identification and where appropriate remediation of any contamination of the land.
- 3.4.12 On the Proposals Map as part of the Carlisle District Local Revised Redeposit Draft 2001-2016, the site is not coincident with any specific land use designation or policy. Rather it is confirmed as being within open countryside, beyond the identified/ appreciable boundary of any recognised settlement.

4.0 The Proposal

4.1 Location – Sustainability

- 4.1.1 The site, as a rural tourism initiative, close to the line of Hadrian's Wall lies within open countryside. Nonetheless, its evidential closeness to the local service centre of Monkhill, renders the site relatively sustainable and indeed, represents the best location option for the proposal. It is anticipated that the majority of visitors to the site will arrive on foot or bicycle. Furthermore, the Hadrian's Wall public bus passes immediately adjacent to the site.
- 4.1.2 The development is wholly geared for catering for walkers, cyclists and visitors to the Wall and its proximity to the Wall is therefore key. The site is well related to the world renowned visitor attraction that is Hadrian's Wall whose visitor attraction contributes to the tourism economy of Cumbria. Likewise, a balance has been struck between a site that is well-related to the Wall but is not wholly detached from the village of Monkhill which is identified in the Local Plan Revised Redeposit Draft 2001-2016 as being a sustainable settlement.
- 4.1.3 Again, development in the open countryside should not be visually intrusive and the topography and screening on the site ensure that this will not be the case. Accordingly, a balance is struck between these issues which renders the scheme appropriate to the character and economic prosperity of the District.

4.2 The Tourism Industry – Rural Diversification and Economic Development

- 4.2.1 Central Government has made clear its support for regarding the diversification of the rural economy as a national imperative. Specifically, with regard to Hadrian's Wall, this is borne out through work being undertaken by Hadrian's Wall Heritage Ltd
- 4.2.2 HWHL is funded by four agencies: One North East, North West Development Agency, English Heritage and Natural England, with differing requirements to fulfil parts of the role outlined above. It is recognised that it is the development of the sum of these parts which will create a compelling leading destination for Britain offering inspirational heritage and spiritually uplifting outdoor educational and tourism based visits creating the twin benefits of jobs and spend for local communities.
- 4.2.3 Recently, the local public house in Monkhill, The Drovers Rest, was representative of a national trend affecting rural pubs and was closed for some time due to lack of trade and a downturn in the economy. The development of the site, in close proximity to the public house, is one way in which development of the site will contribute to the vitality of the area.

4.3 Hadrian's Wall As A Visitor Attraction

- 4.3.1 Hadrian's Wall is the most important monument built by the Romans in Britain. It stretches seventy-three miles from Wallsend near Newcastle, across the neck of England to Bowness-on-Solway in North West Cumbria, and stands today as a reminder of the past glories of one of the world's greatest empires. It has long been recognised that Hadrian's Wall attracts many visitors to the area and in turn, contributes to the economy of the District.
- 4.3.2 The Hadrian's Wall Economic Impact Study, Final Report, March 2004 identifies that the 585,000 visitors to Hadrian's Wall in 2003 is expected to rise to 1,218,000 by 2011, a rise of approximately 208.2% over a period of eight years. The promotion of the feature by English Heritage, Visit Cumbria, Local Authorities and a host of other agencies represents the importance of this regional asset.

4.4 The Siting Of The Manager's Dwelling

- 4.4.1 The site lies out with any settlement boundary. In accordance with national planning guidance, the planning policies in the Carlisle District Local Plan Revised Redeposit Draft 2001-2016 state that development in the undeveloped open countryside will only be permitted where they can be adequately justified for occupation by an agricultural or forestry worker.

- 4.4.2 Policy H7 (Agricultural and Forestry Need) of the Carlisle District Local Plan Revised Redeposit Draft 2001-2016 states that:

"Within the remainder of the Plan area, outside areas covered by Policy H1 and Proposal H156, permission will not be given for dwellings, except where applications are supported by a proven agricultural or forestry need. The size of dwelling should be commensurate with the scale of the business to which it relates.

Section 106 agreements will be used to ensure that such dwellings are only occupied by those working in agriculture or forestry."

- 4.4.3 However, this Policy is superseded by the Planning Inspector's decision letter dated 7th April 2008 following the Local Plan Inquiry where he recommends that the Policy title should be expanded to read "Agricultural, Forestry and Other Occupational Dwellings".

- 4.4.4 Furthermore, the text of the Policy has also been amended to read:

"Outside of those areas covered by Policy H1 and Proposal H16, permission will not be given for dwellings other than those essential to

agriculture, forestry or any other rural-based enterprise and supported by a proven need."

4.4.5 Finally, the third sentence of the Policy has been amended to read:

"Occupancy conditions will be used to ensure that such dwellings are only occupied by those working in agriculture, forestry or any other rural-based enterprise".

4.4.6 The application seeks planning consent for the siting of a dwelling that will be occupied by the site manager. Annex A of 'Good Practice Guide For Tourism' discusses this issue at length in paragraph 24. Here it is made clear that a residential managerial presence is often essential, to achieve quality service to the customer, security for the site, and to meet the obligations of health and safety regulations. Clearly it is preferable to find existing accommodation locally but where this is not feasible, the guidance does allow for the principle of provision of on-site accommodation.

4.4.7 It is key to the facilitation of the services on the site to provide and maintain the facilities for visitors to the site. The applicant currently resides in Maryport and not only would it be unsustainable but it would be unreasonable to travel to and from the site each day to enable the site to operate during the proposed hours.

4.4.8 The sale of the dwelling in Maryport is critical to the funding of the scheme. It is therefore unrealistic to purchase a property closer to the site given the market value of such properties. Notwithstanding this, living away from the site would not enable the continued maintenance and service which will be provided.

4.4.9 The service cabin includes a refreshment area where food and drink will be served to residents on site. It is anticipated that this facility can also be extended to passing walkers and cyclists. Information will be available in the form of leaflets and brochures on other attractions and facilities associated with the Wall.

4.4.10 The applicant is willing to accept that a dwelling in this location is contrary to current planning policies; however, there is growing support for development proposals that support the rural economy and this is borne out by the Planning Inspector's amendments to the wording of policies. The site is not totally unrelated to the village of Monkhill, being approximately 215 metres to the north, a village that is identified as being a sustainable location in the Local Plan Revised Redeposit Draft 2001-2016. The nature of the application is unique and it would be wholly appropriate and accepted that in order to safeguard the undeveloped open countryside, an occupancy condition be attached to the dwelling in order that it remains as part of the business.

4.5 Landscape Impact

- 4.5.1 Reference is made on the submitted block plan and Tree Survey also accompanies the application. Broadly, the site nestles excellently within the topography of the site and the attendant landscaping that surround and structure the site to the extent that only glimpses are afforded from the public highway. In view of this, together with the additional landscaping proposed along the northern boundary, the proposal will not adversely affect the character or appearance of the area.

4.6 Highway Safety and Accessibility

- 4.6.1 The site is located to the east of the Monkhill to Beaumont Road, approximately 215 metres north of Monkhill. A vehicular access exists in the south-west corner of the site. It is proposed to upgrade this existing access to allow for the safe passage vehicles into and out of the site. The improved access and the maintenance of the boundary hedge will further improve highway safety for users of the highway and those using the site. This is a view shared by Mr Pieter Barnard of Cumbria Highways through preliminary discussions prior to the submission of the application. The anticipated level of vehicular traffic will be relatively low due to the fact that the development is intended to primarily attract walkers and cyclists travelling along Hadrian's Wall.

4.7 Biodiversity and Drainage

- 4.7.1 The site is remote from the sewerage network and the site will be drained using a package treatment plant and a soakaway as shown on the submitted block plan.
- 4.7.2 Solar panels will be incorporated on the buildings to supplement the electricity required on site. Furthermore, the scheme will also include provision for the retention of grey water for reuse on site. The road will be constructed from recycled plastic mesh and in filled with gravel. Over time, it is anticipated that grass will grow through the road, further minimising the visual impact, but will be maintained to distinguish this from other areas of the site. Additional planting along the northern boundary of the site will further contribute to the wildlife habitat in the area.
- 4.7.3 Consequently, the proposal is not likely to have a demonstrable harmful impact upon the local environment and its wildlife. A satisfactory balance is struck between sustaining biodiversity and realising the reasonable expectations of this rural business.
- 4.7.4 A main water pipe belonging to United Utilities crosses the site in a north-south direction, approximately 5 metres from the western boundary in the southern corner to 0 metres at the northern end. The site layout has been designed to take this into account and following

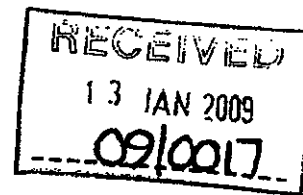
discussions with United Utilities, who require an easement of x metres, there is no objection in this regard.

4.8 Archaeology

- 4.8.1 The site lies within the buffer zone of Hadrian's Wall World Heritage Site. In recognition of this designation, preliminary discussions have been held with both Mr Jeremy Parsons and Mr Gerry Martin. Mr Martin undertook a archaeological desk-based assessment and archaeological evaluation of the site and compiled a subsequent report dated 30th May 2008.
- 4.8.2 The summary of the report identifies a probable mid to late Bronze Age field system comprising a single course of northeast-southwest aligned segmented ditches. It is further stated that no other significant cultural deposits were encountered apart from the deposition of spoil arising from the construction of the Carlisle Canal circa 1830 at the northern end of the site.
- 4.8.3 The layout of the proposed development has taken into account the archaeological constraints of the site. A single service trench will be located within the site, approximately 5 metres from the western boundary. The soakaway will be sited adjacent to the eastern flank.

5.0 Conclusion

- 5.1** The proposal represents a well-founded important rural tourism initiative that is in keeping with, is sensitive to, and otherwise would not cause undue demonstrable harm to interests of acknowledged importance.



Design And Access Statement

Proposal : Change of use of agricultural land to holiday accommodation comprising of 3no. self-catering units, 10no. tent pitches, access road, alterations to existing vehicular access and construction of 1no. dwelling for occupation by site manager

Location: Land at Field No 4490, Monks Hill, Carlisle

Applicant: Mr M Doherty

1.0 Assessment

- 1.1 The proposal seeks to change the use of this agricultural land by providing a short-term holiday chalet complex, comprising of 2no. timber cabins, provision for 10no. tent pitches and a service building providing catering facilities and wash facilities. The application also seeks consent for the siting of a timber cabin for occupation by the applicant. Access will be via the existing Monkhill to Beaumont road. It is expected that the Local Planning Authority will attach suitable conditions to any approval, specifying the terms of occupancy and prohibiting the use of the holiday cabins for permanent occupation as 'dwellings'.
- 1.2 An existing vehicular access exists in the south-west corner of the site. This access serves the existing agricultural use of the land and it is proposed that this access be widened and upgraded as part of the development.
- 1.3 The site layout, as described in detail on the submitted drawings, would occupy relatively flat land, that rises towards the northern boundary. The land is framed by existing established and mature hedgerows that is bounded by open countryside. The layout of cabins, tent pitches and parking takes account of the physical constraints of the site together with the archaeological restrictions imposed on the site. Additional landscaping is proposed along the northern boundary of the site.
- 1.4 The relevance of planning policies are discussed at length in the Supporting Planning Statement.

2.0 Involvement

- 2.1 Prior to submission, the applicant undertook preliminary discussions and sought advice through the Cumbria Rural Enterprise Agency's 'Rural Planning Facilitation' service and with Mr Colin Godfrey, Assistant Development Control Officer, Carlisle City Council. The response was positive to pursue an application for the development of the site for tourist accommodation.
- 2.2 Discussions have also been held with:

Mr Charles Bennett, Landscape Officer, Carlisle City Council
Mr Pieter Barnard, Development Control Engineer, Cumbria Highways
Mr Jeremy Parsons, County Archaeologist, Cumbria County Council
Mr Gerry Martin,
Percolation Tests
United Utilities
- 2.3 As described below, encouragement has been given from the aforementioned agencies/ persons, which has fortified the our resolve

and commitment to providing a high quality, sensitive development, as well as our motivation to develop, strengthen and enhance the proposed rural facility.

3.0 Evaluation

- 3.1 Development on the site has been constrained following consultation with the aforementioned consultees. The site layout, as described in detail on the submitted drawings, would occupy relatively flat land, that rises towards the northern boundary. The land is framed by existing established and mature hedgerows that is bounded by open countryside. The layout of cabins, tent pitches and parking takes account of the physical constraints of the site together with the archaeological restrictions imposed on the site. Furthermore, a United Utilities water pip exits close to the western boundary and development has been eased to the east to allow for any possible future access or maintenance that may be required. Additional landscaping is proposed along the northern boundary of the site.

4.0 Design

- 4.1 The buildings are single storey and are of timber construction. The use of natural materials in the construction process will ensure that the development respects the rural characteristics of the site and
- 4.2 The design, layout and fenestration of the buildings is minimalist to ensure a rustic and practical development that will provide an attractive development but will provide quality accommodation. This is preferable to more visually intrusive forms of tourism development such as static caravans which can be incongruous within the landscape.
- 4.3 The use of natural resources balances other features found within the development such as the use of solar panels, energy efficient light bulbs and the use of grey water.

5.0 Use

- 5.1 The development will provide tourist accommodation on a greenfield site located in open countryside. In this location, planning policies are generally restrictive of development unless material considerations indicate otherwise.
- 5.2 It is further stated that there will be occasions where tourism related development is sought in a location that will be difficult to achieve the objectives of accessibility to sustainable modes of transport. The choice of location, for example, having been chosen for functional need. Fundamentally, however, planners have to weigh up the benefits

from the tourism development against any disadvantages arising from its location.

- 5.3 The reasoning and purpose of this application is to provide tourist accommodation on a site that is located close to Hadrian's Wall. The development of the site will contribute to the expansion of tourist related facilities found close to the entire length of the wall and in turn, will contribute to the overall economy of the District.

6.0 Amount

- 6.1 The application seeks planning consent for the siting 3no. self-catering units, 10no. tent pitches, access road, alterations to existing vehicular access and construction of 1no. dwelling for occupation by site manager.
- 6.2 The development of the site is modest yet these buildings and tent pitches will provide high quality accommodation for tourists on a short-term basis. The scale of the development is sufficient to allow for a viable rural tourism development whilst complementing the rural location and the natural appearance of the site.

7.0 Layout

- 7.1 The layout has been designed to incorporate the constraints that development of the site presents. The site is located close to Hadrian's Wall and potentially presents archaeological issues. In recognition of this designation, preliminary discussions have been held with both Mr Jeremy Parsons and Mr Gerry Martin. Mr Martin undertook a archaeological desk-based assessment and archaeological evaluation of the site and compiled a subsequent report dated 30th May 2008.
- 7.2 The summary of the report identifies a probable mid to late Bronze Age field system comprising a single course of northeast-southwest aligned segmented ditches. It is further stated that no other significant cultural deposits were encountered apart from the deposition of spoil arising from the construction of the Carlisle Canal circa 1830 at the northern end of the site.
- 7.3 The layout of the proposed development has taken into account the archaeological constraints of the site. A single service trench will be located within the site, approximately 5 metres from the western boundary. The soakaway will be sited adjacent to the eastern flank.
- 7.4 Further to the archaeological issues, a mains water pipe is located approximately 0.5 metres east of the western boundary. Accordingly, sufficient distance has been afforded between this infrastructure and

the proposed development to allow future maintenance, should this be required.

- 7.5 The layout of the buildings respects the constraints of the site and also utilise the natural low-lying topography of the site together with the natural landscaping all of which minimise the visual impact of the development. The elevated section of land to the north of the site will remain undeveloped but instead, will be landscaped to supplement the existing site boundaries.

8.0 Scale

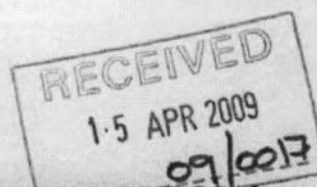
- 8.1 The site lies within the open countryside. The buildings are single storey in scale that will and utilising the topography of the land will minimise the visual impact and will be sympathetic to the existing site boundaries.

9.0 Landscaping

- 9.1 The site is naturally well screened by existing landscaping. It is intended to retain the existing boundaries that comprise of traditional hedgerows and to supplement these by additional landscaping along the northern boundary consisting of ????

10.0 Access

- 10.1 The intention from the outset has been to create a facility that will be accessible and life-enhancing to use, both by the able-bodied and those who are less mobile. The access road will provide level and stable access into the site and the timber deck will provide level access into the building. The accommodation has been planned to enable unimpeded wheelchair access internally. All facilities including toilets, electrical sockets etc. are also accessible.



REPORT ON THE PROPOSED SEWAGE TREATMENT AND DISPOSAL
SYSTEM AT MONKHILL, CARLISLE IN COMPLIANCE WITH DETR
PLANNING CIRCULAR 03/99, THE BUILDING REGULATIONS
(ENGLAND & WALES) 2002 AND THE WATER RESOURCES ACT 1991 (AS
AMENDED BY THE ENVIRONMENT ACT 1995)

CLIENT: Martin Doherty and Julie
Ballantyne, 2 Crosby
Moor, Crosby, Maryport, CA15
6RS

SITE ADDRESS: Field No. 4490 Monkhill

CATCHMENT: Groundwaters

NATIONAL GRID REFERENCE: NY34440 58910

PLANNING APPLICATION No: 09/0017

LOCAL AUTHORITY: Carlisle City Council

The applicants are proposing to establish a modest tourism development comprising of a number of wooden cabins and tents which will be restricted to bed and breakfast only.

The site is remote from the public sewerage network and therefore a private system of sewage treatment and disposal will be required.

The sizing of the treatment plant is based on the organic load generated by the maximum number of people that can be expected to be accommodated at any given time.

The applicants have already undertaken research into the likely numbers of people using the site and the occupancy rates are shown as Appendix 1

Using this information and the sizing criteria contained in the British Water Code of Practice 2005, the size of the treatment will be as follows:

1 No. Owners cabin with an occupancy of 2. This is classed as **5 people** for the purpose of the sizing of the treatment plant. Each person will generate 60 gms of BOD₅ (atu)/day which gives an organic load of **300gms BOD₅(atu)/day**

2 No. timber Shielings with a potential occupancy of 4

= $2 \times 4 \times 80\%$ anticipated maximum occupancy = **6.4 people**. Each person will generate 30gms BOD₅(atu)/day which gives an organic load of **192 gms BOD₅(atu)/day**.

3 No. timber Wigwams with a potential occupancy of 4 = $3 \times 4 \times 80\%$ anticipated maximum occupancy = **9.6 people**. Each person will generate 30gms BOD₅(atu)/day which gives an organic load of **288 gms BOD₅(atu)/day**

10 No. Tents with a potential occupancy of 2 people = $10 \times 2 \times 80\%$ anticipated maximum occupancy = **16 people**. Each person will generate 30 gms BOD₅ (atu)/day which gives an organic load of **480 gms BOD₅ (atu)/day**.

Therefore the total organic load generated by the site = **1,260 gms BOD₅(atu)/day**. This equates to 21 people including the owners cabin and this is the population for which the treatment plant must be sized for.

Given the wide variability of loadings that can be expected from the site it is recommended that an Extended Aeration Activated Sludge process be deployed and details of the DMC6 are shown as Appendix 2. It is not recommended that the treatment plant be purchased through a builders merchants. The plant should be purchased through the local WPL specialist stockist who will install, commission and maintain the plant in compliance with the mandatory requirements of the Environment Agency.

Turning to the final disposal of the fully treated effluent, the site does not contain a suitable watercourse to discharge into. It therefore follows that the only receptor available is Groundwaters via a piped infiltration soakaway.

Guidance on the practice for the design and installation of drainage fields for the use in Wastewater treatment is provided in BS 6297: 2007 + A1:2008

On 17 March 2009, number of test holes were carried out along the Eastern edge of the development site Appendix 3

The porosity testing failed due to the presence of boulder clay beneath the 350mm depth of topsoil.

The clay band was found to be 1350mm deep beneath which sand and gravel was encountered.

Contact was made with the Environment Agency's Groundwater Protection Team in Warrington and a copy of that consultation is shown as Appendix 4.

The average V_p of the sand and gravel was found to be 22.6 seconds \times mm^{-1} and the total area (A_t) of the soakaway is calculated from the formula $A_t = V_p \times P \times 0.2$ where $V_p = 22.6$, P = Population Equivalent (12) and 0.2 = constant of 20% for fully treated sewage effluent.

Therefore, $A_t = 54.2\text{m}^2$. Assuming that the soakaway trenches are 900mm wide then the total length of trenches = 60.3m.

The soakaway may be divided into two 30 metre lengths as shown as Appendix 5.

The impermeable 1350mm depth of boulder clay will need to be replaced by imported sand with a porosity value of 15-20 seconds \times mm^{-1} .

In terms of the volume of sewage generated by the whole site for consenting purposes, this is calculated as follows:-

Owners cabin = 2.5 people \times 180 litres/capita/day = 450 litres.

Shieldings, Wigwams and Tents have been assessed at 50 litres/capita/day = 32 \times 50 litre = 1,600 litres

Total = **2,050 litres** which attracts the lower application fee of £124 and is also below the threshold for annual charging by the Environment Agency.

The application to discharge sewage to Groundwaters via a soakaway will take up to 4 months for the Environment Agency to determine.

SIGNED: *D. Clayton*

DATE: 6th April 2009

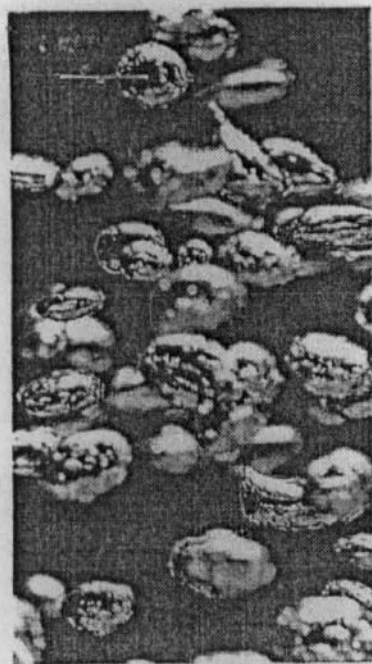
Mr. D Clayton FCIWEM Dip CWEM C.Env
Chartered Water & Environmental Manager, Chartered Environmentalist
Lakeland Environmental Management
Fell Cottage, Plumpton, Penrith
Cumbria
CA11 9PF

Roman Wall Lodges – Monkhill, CarlisleOccupancy Rates

Description	No.	Max Occupancy per building	Expected Occupancy per building	Max Total	Expected Total
Residence (owner)	1	2	2	2	2
Shieling (shower & wc)	2	4	2	8	4
Wigwam (no shower/wc)	3	4	2	12	6
Tent	10	2	1	20	10
Total				42	22

Note:

- Maximum occupancy rates based on capacity of building/Expected occupancy rates based on information supplied by Cumbria Tourism and Hadrian Wall Partnership
- Occupancy rates for tents will be restricted to 20 people max.
- Residents in Wigwams and tents will have access to 2 shower/wcs based in Services block
- Breakfast will be prepared and served in Services block – we anticipate a maximum of 15 breakfasts to be served daily
- High season will be 210 days (April – October) and we anticipating building from 60 – 80% occupancy over 3 years.
- Low season will be 155 days (November to March) and we anticipate an occupancy rate of 20%.



The Diamond

The WPL Diamond sewage treatment range is specifically designed for domestic properties situated away from waste water mains drainage.

The Diamond is CE marked and type tested in accordance to EN12566-3 2005, the new European standard covering wastewater treatment plants, making it fully compliant with UK Building Regulations.

Utilising a unique design, each purposely sized plant offers high quality, discreet water treatment whilst ensuring value for money, low ongoing running costs and complete peace of mind.

Benefits

- Outstanding value for money
- Continuous bacterial digestion giving up to 5 year desludging intervals
- Easy installation – unique design minimises labour and materials
- Low running cost of the small, quiet air blower
- Robust construction reduces the need for concrete backfill
- Compact design does not require large plant for excavation or handling
- Reduced soakaway
- Irish Agrément Certified
- No odours as the process does not require primary settlement
- Low visual impact – small lid, flush with the ground
- Reliable operation – simple design, no internal moving parts
- Excellent performance meeting normal environment agency consent standards
- Over 40,000 units installed world wide
- Five year warranty and three years servicing consumables

Model Range & Flow Rates

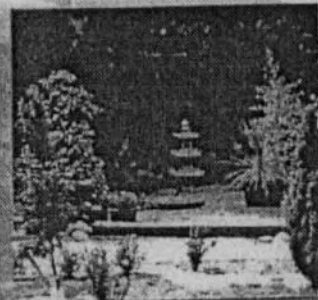
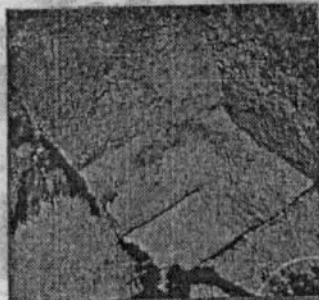
Model	DMS2	DMS3	DMS4	DMS5	DMC6	DMC7	DMC8	DMC9
Population range (persons)	1-6	5-11	10-15	14-20	21-27	28-35	36-45	46-55
Maximum organic loading BOD/day (grams) A WPL/British Water Loading Guide providing further information for non-domestic applications is available	360	660	900	1200	1620	2100	2700	3300
Maximum average daily flow (litres) *	1200	2200	3000	4000	5400	7000	9000	11000
Maximum blower power consumption (kWh) ** (depending on blower model)	0.12	0.14	0.17	0.24	0.55	0.75	0.75	0.75

* Peak flow must not exceed 3 x total flow for more than half an hour in any two hour period.

** Compressor manufacturer's data is an approximation to plant conditions.

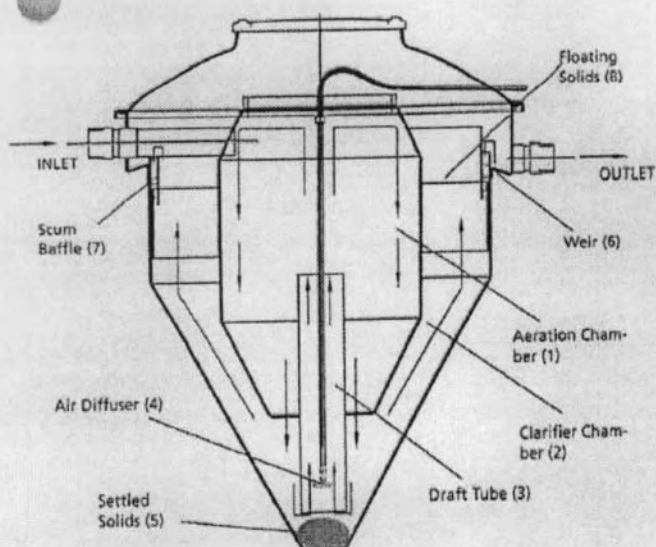
Process performance is subject to strict adherence to WPL's installation, operation & maintenance manuals, user guide and a start up period of 6-12 weeks depending on plant loadings and water temperature. WPL provides a full brochure that details consents to discharge and how to calculate whether your soakaway area or discharge point (drainage ditch or stream) will meet the requirements. Please contact us, or your local distributor, for more information.

Stepped excavation minimises the amount of backfill material required and the Diamond's lightweight design allows for ease of movement around the site. For installations where there is a high water table, additional fixing anchors are available. Once installed, the Diamond provides unobtrusive and odour free waste water treatment.



Process Description

- The central aeration chamber (1) is a circular tank with a sloped open base, which empties into the bottom of the clarifier chamber (2).
- Located in the centre of the aeration chamber is a 200mm diameter draft tube (3), which extends to 100mm from the bottom of the clarifier. Air is released at the bottom of the draft tube through a disc plate diffuser (4).
- As the diffused air rises in the draft tube it causes an upward flow of process fluid. This draws the settled solids (5) from the bottom of the clarifier up through the draft tube where they are discharged at the surface of the aeration chamber.
- The design of the draft tube ensures continuous and complete mixing of oxygen with the sewage. This oxygenation allows the various aerobic organisms that biologically degrade the waste water contaminants to grow.
- Gravity causes the aerated solids to settle at the bottom of the tank where they are again drawn back up through the draft tube.
- As raw sewage enters the aeration chamber, it displaces biological solids from the aeration compartment to the clarifier. Less turbulent conditions in the clarifier allow the digested solids to settle to the base where they are returned back to the aeration compartment.
- The clarified (treated) effluent flows slowly up through the clarifier and over a weir (6), which extends around the periphery of the tank.
- The effluent collects in an outer trough where it is discharged through a 110mm pipe connection. The scum baffle (7) located inside the overflow weir prevents floating solids (8) from passing over the weir.



Process Performance

The Diamond is designed to "clean" the wastewater to a standard of 20mg/l Biological Oxygen Demand (BOD) and 30mg/l Suspended Solids (SS) on a 95 percentile basis. Additional filtration equipment is available to extend the treatment process to meet standards as high as 10mg/l BOD and 15 mg/l SS. More stringent environmental requirements involving ammonia and phosphorous reduction can also be accommodated, please contact WPL for details.

WPL also provides a data sheet to assist you with soakaway construction.

Technical Data and Dimensions

Model	Outside diameter	Height to inlet	* Inlet invert depth	Height to outlet	In ground depth	Weight empty	Total capacity
DMS2	1.85m	1.69m	570mm	1.59m	2.26m	120 Kg	2271 LTR
DMS3	2.1m	1.85m	700mm	1.73m	2.55m	160 Kg	3028 LTR
DMS4	2.1m	2.04m	700mm	1.92m	2.74m	210 Kg	3974 LTR
DMS5	2.1m	2.04m	700mm	1.92m	2.74m	210 Kg	3974 LTR
DMC6	3.2m	2.67m	590mm	2.57m	3.26m	380 Kg	9056 LTR
DMC7	3.2m	2.67m	590mm	2.57m	3.26m	380 Kg	9056 LTR
DMC8	3.2m	3.37m	590mm	3.27m	3.96m	460 Kg	15038 LTR
DMC9	3.2m	3.37m	590mm	3.27m	3.96m	460 Kg	15038 LTR

* Deeper Inverts can be accommodated with our standard invert extensions; contact WPL for further information.

Blower Installation

Each blower is supplied with an enclosure to protect it from outdoor conditions. It should be connected to a single-phase supply (240v) via a suitable IP55 rated weatherproof socket or fused spur (not supplied) by a competent electrician. Included is 10m of airline to connect between the blower and the tank, however, blower installations of up to 30m from the tank can be accommodated. Please contact WPL or your authorised distributor for advice.

Service & Maintenance

The system requires less than two hours annual maintenance. As standard, it is sold with three years supply of service consumables. Service and maintenance agreements are available from WPL. Typical domestic systems will need to be partially desludged every 3-5 years. Systems that receive their design loading may require desludging every 1-2 years.

WPL is recognised internationally as a principle designer and manufacturer of high quality package and turnkey sewage treatment systems, and grease management solutions.

It has over 14 years' industry experience and services an extensive customer portfolio comprising the UK's major water utilities, all areas of the commercial sector and the domestic market place.

The company's consistent focus on ecology, coupled with extensive research & development into new technology, have enabled it to far exceed all existing and proposed discharge consent standards enforced by the Environment Agency, SEPA and other regulatory authorities.

Delivery

Deliveries within mainland England, Scotland & Wales can normally be guaranteed within 15 working days. Units are shipped on a grouping arrangement and deliveries to some regions may take longer.

Guarantee

The Diamond sewage treatment range comes with a comprehensive five year parts and workmanship guarantee on the blower and plant respectively.

The unit's sturdy Glass Reinforced Plastic (GRP) construction and WPL's quality assurance procedures permit a minimum life expectancy of 25 years.

Your local distributor is:



**BORDER WATER
TECHNOLOGIES**

Accredited Specialist Installer of Waste Water Treatment Systems

River House
Larrie Salkeld
Penrith
Cumbria CA10 1NJ

Phone/Fax: 01768 881612
Mobile: 07831 742828
markennion@btiscall.co.uk

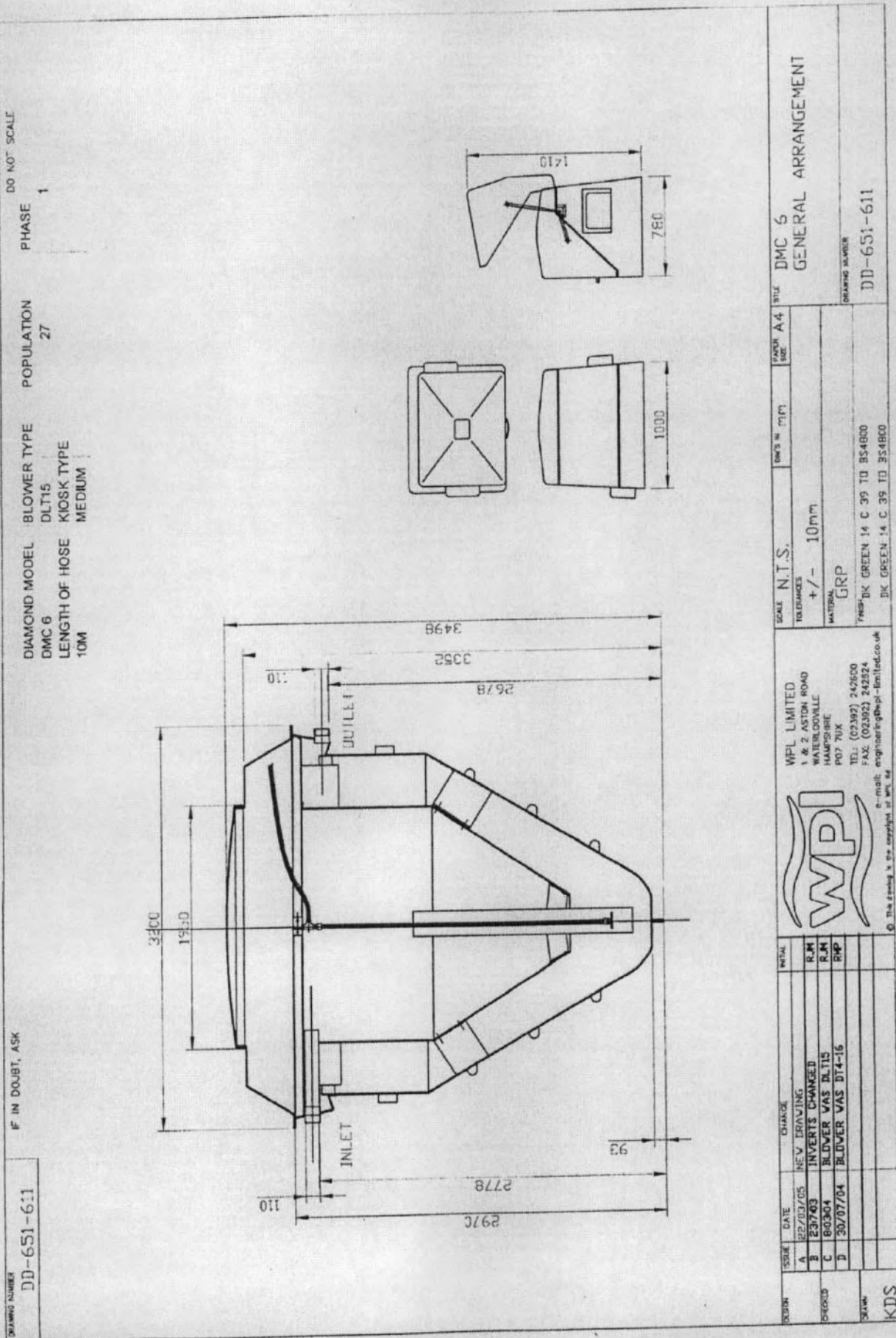
Mark Ennion

WPL Limited
Units 1 & 2 Aston Road
Waterlooville
Hampshire PO7 7UX
United Kingdom.

Tel: +44 (0) 845 4504818
Fax: +44 (0) 23 92242624
email: domestic@wpl.co.uk
www.wpl.co.uk



WPL Limited registered in England & Wales NI 2583411 VAT No. 558 3801 15. WPL Limited has a policy of continual product development and the above information may be subject to change without notice. WPL Limited are fully compliant with the British Water Code of Practice.



Diamond - DMC 6

Just how quiet is the air blower?



•	DMS1	36 DbA
•	DMS2	38 DbA
•	DMS3	42 DbA
•	DMS4	44 DbA
•	DMS5	45 DbA
•	DMC6	61 DbA
•	DMC7	65 DbA
•	DMC8	65 DbA
•	DMC9	65 DbA

- The above figures are measured at 1.0m from the blower, which was blowing to atmosphere.
- The blower will be significantly quieter installed in its housing and with a hose attached.
- Compare the noise of a DMS2 blower to "the humming of a freezer motor"

How much does the blower cost to run?

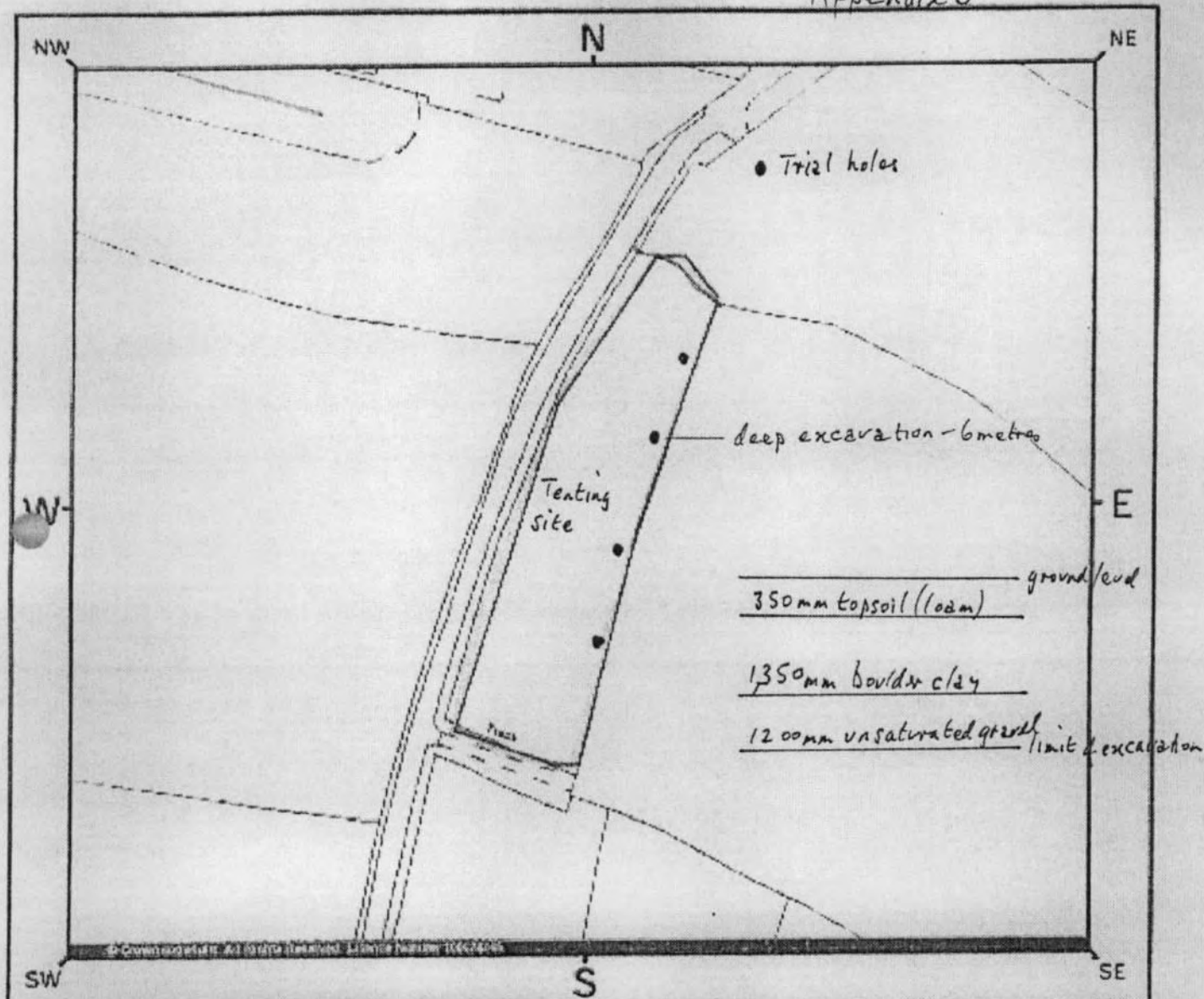


•	DMS1	£40.43 year	0.071 kW/h
•	DMS2	£54.09 year	0.095 kW/h
•	DMS3	£65.48 year	0.115 kW/h
•	DMS4	£71.18 year	0.125 kW/h
•	DMS5	£119.57 year	0.210 kW/h
•	DMC6	£313.17 year	0.550 kW/h
•	DMC7	£427.05 year	0.750 kW/h
•	DMC8	£427.05 year	0.750 kW/h
•	DMC9	£427.05 year	0.750 kW/h

- The above costs assume 8760 hours run per year and an electricity cost of £0.065 per unit.
- The kW/h are manufacturers actual figures.

Location Map

Appendix 3

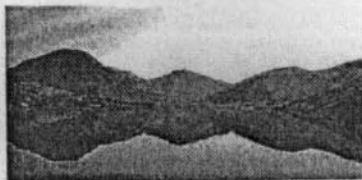


Site

Land at Monkhill Farm
Monkhill Farm
Burgh-by-Sands
Carlisle
Cumbria
CA5 6DB

Grid Reference

Easting 334440
Northing 558910



Lakeland Environmental Management

Mr. D Clayton FCIWEM Dip. CIWEM C.Env.
Chartered Water & Environmental Manager, Chartered Environmentalist

F A O Darren Billington (Groundwater Protection Team)
Environment Agency
Richard Fairclough House
Knutsford Road
Warrington
WA4 1HG

23rd March 2009

Dear Darren

RE: TESTING SITE, MONKHILL, CARLISLE NGR NY 34440589910

I am currently acting for a client who is proposing to establish a small seasonal camping site of 15 tents and a house at Monkhill near Carlisle.

I undertook porosity testing of the land last week for the purpose of identifying suitable ground for the final disposal of fully treated effluent.

The ground was found to be unsuitable for a conventional piped infiltration system due to the presence of boulder clay.

A local agricultural drainage contractor undertook the trial holes for me and you will see from the attached sketch of the profile of the deep observation hole that sand and gravel was encountered at a depth of 1.7 metres. I have kept the holes open for a number of days and no water has entered.

The agricultural contractor who also farms locally has told me that the profile is fairly typical for the area.

It will be possible to replace the boulder clay with sand of say a V_p of 15-20 seconds \times mm^{-1} in order to form distribution trenches for a piped infiltration soakaway.

I would be very grateful to you for your view on this proposal given that there are no alternative disposal routes such as a suitable local watercourse.

Thank you in anticipation.

Yours sincerely

D Clayton

Enc

David Clayton

From: David Clayton [David@LakelandEnviron.eclipse.co.uk]
Sent: 29 March 2009 11:18
To: 'Billington, Darren'
Subject: RE: Testing site, Monkhill, Carlisle

Hello Darren

I am very grateful for your helpful comments.

As a precaution, I have extended one of the test holes to a total depth of 6 metres bgl and there is no evidence of water at this depth. The composition changes slightly towards a greater percentage of sharp sand..

I will now submit an application for consent based on these findings.

Regards,

David

-----Original Message-----

From: Billington, Darren [mailto:darren.billington@environment-agency.gov.uk]
Sent: 26 March 2009 09:46
To: David@LakelandEnviron.eclipse.co.uk
Subject: Testing site, Monkhill, Carlisle

Hi David,

Thanks for sending through the letter dated 23rd March regarding the above site.

Your proposal to replace the clay with appropriate sand to enable the hydraulic functioning of the system appears reasonable. The main objective from a groundwater perspective is to ensure that there is an adequate unsaturated zone beneath the invert of the drainage field, and this appears to be confirmed from your observations. One metre is typically the accepted minimum and seasonal fluctuations need to be considered.

You do not mention the likely discharge volume, but I would expect it to be fairly low from what you have described. So this is not anticipated to create further issues.

Let me know if you have any further queries

Kind regards
Darren.

Darren Billington
Technical Officer 1 (Groundwater & Contaminated Land)
Internal Tel : 7 21 3317
External Tel : 01925 543317
Fax : 01925 852260
email : darren.billington@environment-agency.gov.uk

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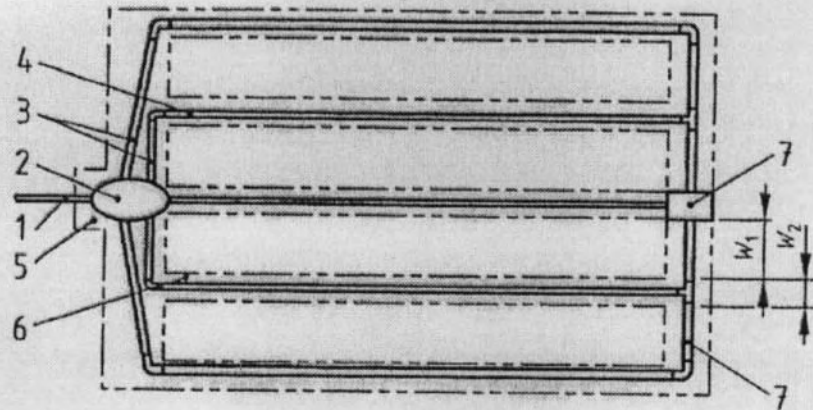
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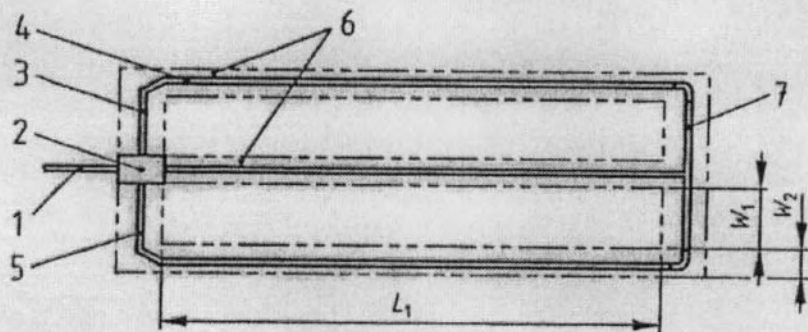
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Appendix 5

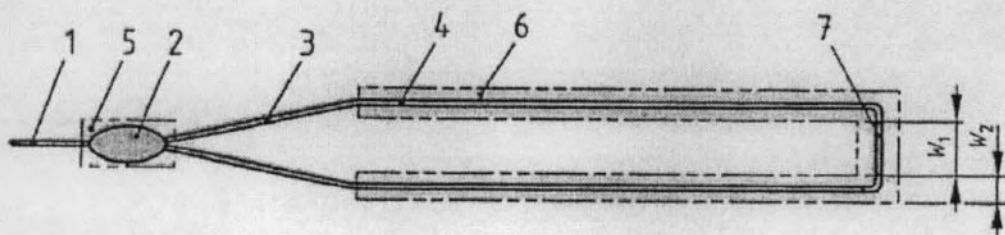
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b)



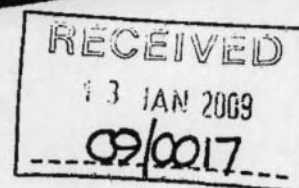
c)



Key

a)	Five trenches	5	Sand layer, if necessary to level distribution chamber and pipework
b)	Three trenches	6	Distribution layer
c)	Two trenches	7	End connection with ventilation and access, if necessary
1	Connection pipe	W_1	Natural ground width between trenches 1 m
2	Distribution chamber	W_2	Distribution layer width 0.9 m
3	Distribution pipe	L_1	Distribution layer length 30 m
4	Infiltration pipe		

Hadrian's Wall Heritage Ltd



9 January 2009

Martin Doherty
2 Crosby Moor
Crosby
Maryport
CA15 6RS

Dear Martin and Julie

Site visit to proposed Camp Site Development at Monkhill

Further to the recent site visit at Monkhill, I write to confirm that Hadrian's Wall Heritage fully support your proposal in principle, and are keen that the development goes ahead in the near future.

We are particularly interested in the development due to the fact that it will offer high quality, environmentally friendly facilities for walkers and cyclists in an area of Hadrian's Wall corridor that currently has no provision of this nature.

Our organisation is responsible for the management and development of the Hadrian's Wall National Trail and we constantly receive correspondence from visitors asking for more facilities of this nature in the area to the west of Carlisle.

David McGlade, Hadrian's Wall National Trail Manager will write to you separately to support the project with a profile of the types of visitor the National Trail attracts.

We are very keen to work with you all the way through the development of the project, and please use Marie Whitehead as your primary point of contact.

If I can be of any further assistance please do not hesitate to contact me.

Yours sincerely

Martin Walker
Director of Sustainable Development

Colin Godfrey

From: David McGlade [David.McGlade@hadrianswallheritage.co.uk]
Sent: 02 April 2009 17:05
To: Colin Godfrey
Cc: martin-doherty@tiscali.co.uk
Subject: Monkhill Camping Park
Attachments: InterScan_Disclaimer.txt

PLANNING & HOUSING SERVICES	
REF	09/0017
06 APR 2009 12	
RECORDED	KL
SCANNED	
PASSED TO	CG
ACTION	

Dear Colin,

I am writing to you, as the Hadrian's Wall Path National Trail Manager, with some additional information in support of Martin Doherty's development of Monkhill Camping Park.

First of all I can confirm that there still exists a general walkers' complaint about a shortage of short-stay accommodation west of Carlisle, in particular for camping/hostel and budget B&Bs. The situation since the Trail opened in 2003 has improved elsewhere, to the point whereby I am more or less happy with the accommodation provision, with the area west of Carlisle being the last to catch up.

It may be helpful to quote from Natural England's 2007 national survey of all of England's National Trails. The survey revealed that 68% of long-distance walkers using National Trails (of which Hadrian's Wall Path has 7,000, plus several thousand more short-stay visitors) use either B&Bs or camping sites/camping barns/hostels.

The survey also confirmed that long-distance walkers, travelling on foot, quite understandably prefer to see more and cheaper accommodation close to the Trails. Without access to cars walkers also want to stay as close as possible to the route and I would say that Monkhill Camping Park helps to meet that aspiration.

I would also like to highlight the fact that Hadrian's Wall Path is attracting a sizeable constituency of people who are new to walking in the countryside; this can only be a good thing but it is clear from my postbag that these people have their own particular requirements, many of whom prefer not to carry their heavy packs with them whilst on the Trail. This is reflected in the fact that Hadrian's Wall Path now supports three full-time baggage carrying companies who transport walkers' bags every morning between accommodation addresses.

To summarize, most National Trail walkers using Hadrian's Wall Path do not have access to cars, they arrive and leave on foot each day and they rely on local accommodation provision and supporting services to enable their holidays to become a reality. There remains a shortage of budget accommodation, including camping provision, west of Carlisle, and I believe that the Monkhill Camping Park development will help to meet that need.

Yours sincerely,

David McGlade
National Trail Manager
Hadrian's Wall Heritage Limited
East Peterel Field
Dipton Mill Road
Hexham
NE46 2JT

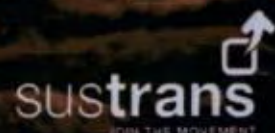
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The Economic Impact of Cycle Tourism in North East England

Executive Summary

April 2007



Introduction

This report presents the findings of a study to estimate the economic impacts of four routes that form part of the National Cycle Network in the North East of England, conducted by Sustrans on behalf of One North East. The report is underpinned by a literature review, an overview of tourism policies, and an analysis of primary data collected on the National Cycle Network by Sustrans between 2001 and 2006.

The executive summary that follows gives a brief overview of the study, highlighting the benefits of these four routes to the economy of North East England. However, the study also presents a case for investment in sustainable tourism initiatives, as both a response to global climate change and to reduced access to long-distance travel as a consequence of the rising price of oil.

Whilst international tourism arrivals across the world are forecast to grow based on past trends, there is some debate as to whether this trend is sustainable. Some argue that the tourism sectors need to be integrated more closely into the framework of sustainable development. This will bring opportunities to destinations that seek to place sustainable development at the core of their tourism offering. Cycle tourism is one such sustainable development, which offers additional potential to build home-grown markets, and in turn represents best practice in carbon neutral tourism development.

Key headlines for the North East region

Four cycle routes in North East England make a notable contribution to the regional tourism economy. The routes; the C2C (Sea to Sea) Cycle Route, the Coast and Castles Cycle Route, Hadrian's Cycleway and the Pennine Cycleway (northern section), form part of the National Cycle Network.

- Those parts of the four routes lying within the North East region attracted 302,000 cycle trips in 2006; the combined total for the whole of the routes (North East and North West region) exceeds half a million
- Route users contributed £9.6 million of direct expenditure to the North East economy in 2006
- This represents a value of £13.4 million to the wider regional economy
- This supports 216 jobs in the immediate vicinity of the routes in the North East
- In 2006 route users from out-of-region visiting the North East generated £5.9 million, supporting 95 full-time equivalent jobs
- Users attribute an additional amenity value of £1.7 million to the routes in the North East
- Cycling activity is important to local supply chains, with major implications for the circulation of income in the local tourism economy



Below: Cycling alongside Hadrian's wall, National Route 72 in Cumbria

Left: Sea to Sea Cycleway in the North East



Image courtesy of

Study context and outline

The research uses a unique approach, applying established, contemporary econometric theories in combination with user-oriented data collection to produce a 'bottom-up' measure of economic impact. This differs from most analyses of tourism activity (and economic activity in other sectors) by virtue of the fact that other research has tended to rely on aggregation methods from a 'top down' perspective that are inherently flawed by the nature of their design. This research therefore represents an internationally significant contribution to researcher and practitioner knowledge, as well as providing an unusually accurate measure of the extent of the impact of cycling activity in the North East region.

Whilst creating the most accurate reflection of cycling in the North East, it is worth reiterating that other tourism activity, unless analysed in a similar way, will be overvalued, perhaps to the disadvantage of cycling.

The current economic policy in the North East of England, recognises the importance of the tourism sector to the regional economy. Within this context and in relation to rural regeneration, tourism strategies highlight cycling as an appropriate tourism offering to develop. The study findings show clearly the extent of demand that exists for leisure cycling in the North East region. They also illustrate the extent to which leisure cycling can play a part in the regeneration of the North East region through attracting out-of-region tourism, stimulating tourism business development, and improving the quality of life for North East residents.

Impacts of each of the key routes

Coast & Castles: 68,000 trips were made on the Coast & Castles Cycle Route in 2006. Of these 8,100 were end-to-end users. The route generated £3,300,000, and created or safeguarded 53 full-time-equivalent jobs.



Image courtesy of

C2C: 241,000 trips were made on the C2C Cycle Route in 2006 (113,000 in the North East and 128,000 in the North West region). Of these 14,000 were end-to-end users. The route generated £10,700,000 and created or safeguarded 173 full-time-equivalent jobs.

Hadrian's Cycleway: 160,000 trips were made on Hadrian's Cycleway in 2006 (81,000 the North East and 79,000 in the North West region). Of these 7,500 were end-to-end users. The route generated £6,500,000 and created or safeguarded 105 full-time-equivalent jobs.

Pennine Cycleway: 39,000 trips were made on the Pennine Cycleway in 2006. Of these 2,100 were end-to-end users. The route generated £1,800,000 and created or safeguarded 28 full-time-equivalent jobs.



The coast at Toss Valley in Cumbria, North West

Picture photo: Llandudno Castle to Rhos Gwynedd, Cumbria

The implications for One NorthEast

One NorthEast has already gained a competitive edge in the development of its cycle tourism product; particularly these four flagship routes, but also the rest of the National Cycle Network in the region, other routes in the region, and mountain-biking provision at a number of locations. Between 2002 and 2006 the total investment in the four routes was just under £2.7m, an average of approximately £500,000 a year. It is estimated that in 2006 alone, the four routes generated £4.2m in tourism income for the region. Forecasts suggest that further development of the cycle tourism product and targeted marketing will enable the region to build on this. There are strong indications that market growth is readily achievable.

Some policy makers are suggesting that tourism is beginning to focus more on near-to-home destinations as personal security and energy prices start to modify market preferences. This provides a great opportunity for the North East, in that it has the potential to build on current demand from out-of-region visitors for cycle tourism. However, intra-regional visitation is also an important part of product-function. In conventional terms this is currently not considered important, but in terms of reduction of leakages in the regional economy (rather akin to the import-export balance), carbon footprinting and quality of life, cycle tourism has considerable benefits. This report acknowledges but does not explore the further benefits to a local user market, such as health enhancing physical activity, social inclusion and accessibility benefits

As well as realising the benefits of supplying a product that provides a response to market changes that are predicted to result from global climate change, the region will also be able to contribute to the global efforts to minimise the effects of climate change by helping to balance the tourism mobility dilemma; the near-to-home tourism market will be encouraged through price and carbon reduction strategies in future. Cycle tourism fits the emerging sustainable development approach well, as it is a low-carbon form of tourism.

Sustrans is the UK's leading sustainable transport charity and works on practical projects to encourage people to walk, cycle and use public transport to benefit health and the environment.

Sustrans, National Cycle Network Centre, 2 Cathedral Square, College Green, Bristol, BS1 5DD
www.sustrans.org.uk Tel: 0117 927 755 Registered Charity No. 328550

SCHEDULE A: Applications with Recommendation

09/0130

Item No: 14

Date of Committee: 29/05/2009

Appn Ref No:
09/0130

Applicant:
Mrs L J Eplett

Parish:
Carlisle

Date of Receipt:
23/02/2009

Agent:
Jock Gordon

Ward:
Denton Holme

Location:
3 Dowbeck Road, Carlisle, Cumbria, CA2 7BX

Grid Reference:
339153 555378

Proposal: Erection Of Timber Decking & Handrails To Front & Side Of Dwelling
(Revised/Retrospective Application)

Amendment:

REPORT

Case Officer: Stephen Daniel

Reason for Determination by Committee:

This application is brought before the Development Control Committee for determination due to the retrospective nature of the application, because of its scale and potential impact on the street scene, and because Committee has refused a previous application at the premises.

1. Constraints and Planning Policies

Local Plan Pol CP5 - Design

Local Plan Pol H11 - Extns to Existing Resid. Premises

2. Summary of Consultation Responses

Cumbria County Council - (Highway Authority): no objections;

Tree/ Landscape Officer (Carlisle City Council): the front corner of the decking must be removed to help to reduce the visual impact.

Planting along the side of the decking, that would blend in with existing hedging in the area would be acceptable. The planting should have an instant impact and will need to consist of 'larger' plants in sufficient numbers.

Planting should also take place in front of the decking adjacent to Dowbeck Road.

SCHEDULE A: Applications with Recommendation

09/0130

Climbers such as honeysuckle and clematis would provide adequate softening and cover.

It would be preferable to replace the bark, that is currently adjacent to the decking with gravel to prevent weed growth.

3. Summary of Representations

Representations Received

Initial:	Consulted:	Reply Type:
1 Dowbeck Road	03/03/09	
5 Dowbeck Road	03/03/09	
7 Dowbeck Road	03/03/09	
8 Dowbeck Road	03/03/09	
10 Dowbeck Road	03/03/09	

- 3.1 This application has been advertised by means of notification letters sent to five neighbouring properties. No verbal or written representations have been made during the consultation period.

4. Planning History

- 4.1 An application to erect timber decking and handrails to the front and side of the dwelling was refused in July 2008.

5. Details of Proposal/Officer Appraisal

Introduction

- 5.1 This is a retrospective application which seeks approval for the erection of timber decking and handrails at 3 Dowbeck Road, Carlisle. The property is a semi-detached brick built dwelling, which is part rendered on the front elevation and has a tiled roof. The property, which is surrounded by residential development, has gardens to the front, side and rear. The dwelling is elevated above Dowbeck Road, which runs downhill from Weardale Road, past the property. A 1m solid wooden fence, with a concrete base and posts, separates the dwelling from the highway.

Background

- 5.2 Members will recall that the application was deferred at the last meeting, in order to give the applicant an opportunity to submit details of a proposed landscaping scheme, which would help to reduce the visual impact of the

decking.

- 5.3 The application was originally refused at Development Control Committee in July 2008 for the following reason:

The proposed decking, by virtue of its scale and siting, would have a detrimental impact on the character of the area. As such, the proposal is contrary to Criteria 2 and 4 of Policy H17 (Residential Amenity) of the Carlisle District Local Plan and Criteria 2 and 4 of Policy CP5 (Residential Amenity) of the Carlisle District Local Plan Revised Redeposit Draft (as amended by the Inspector's Report).

The Proposal

- 5.4 The decking and handrails, which are constructed of timber, are located to the front and side of the dwelling. Due to the change in levels, the height of the decking varies from 0.9m, adjacent to 1 Dowbeck Road, to 1.6m, adjacent to the applicant's own garden. Handrails 1.2m high run along the edge of the decking. The decking projects 3m from the front of the dwelling, where it comes within 1m of the front boundary fence. To the side of the dwelling, the decking projects outwards by 3.7m and sits adjacent to the remainder of the applicant's side garden. This revised application is seeking to remove the front corner of the decking, which lies adjacent to Dowbeck Road, in order to reduce its visual impact. The width of the decking fronting Dowbeck Road would be reduced from 9.8m to 7m, whilst its length would be reduced from 10.3m to 8.3m.
- 5.5 The applicant is also proposing to plant an evergreen hedge to screen the front and side elevations of the decking. A 0.6m high hedge would be planted along the side of the decking and at the front of the decking, in the small gap that remains between the edge of the decking and the front boundary fence. The species, which would be planted 0.3m apart, would grow at approximately 0.3m per annum, and would fully screen the decking within 2 to 3 years of planting. The applicant is also intending to place large specimen plants in pots on the decking, up against the handrails and this would further help to reduce the impact of the decking.
- 5.6 The dwelling is located at a higher level than the road, and sits above the front and side gardens of the property. The applicant has added the decking, which is at the same level as the house, to increase the useable space within the curtilage. A table and chairs and a two seater swing chair were sited on the decking, at the time of the site visit.
- 5.7 Members should note that since the last application was refused, the applicant has added a bark surface adjacent to the decking and placed some planters in the garden, adjacent to the decking. In addition, the applicant has also sought to address concerns, that have previously been expressed, about the small strip of garden that remains between the decking and the front boundary fence. At the time of the last application, large weeds were growing in this area and it looked unsightly. The applicant is proposing to clear this and plant

an evergreen hedge, as outlined above.

Assessment

- 5.8 The relevant planning policies against which the application is required to be assessed include Policies CP5 and H11 of the Carlisle District Local Plan 2001-2016.
- 5.9 The proposal raises the following planning issues:
1. Whether The Proposal Is Appropriate To The Dwelling
- 5.10 Given the height of the decking, together with the handrails, and given that it extends some 3m out from the front of the dwelling, it is visually prominent and looks incongruous when viewed against the brick/ render property. The visual impact would, however, be reduced by removing the front corner of the decking adjacent to Dowbeck Road and planting an evergreen hedge adjacent to the front and side elevations of the decking.
- 5.11 Whilst the decking is prominent, it is not considered to be so visually harmful to warrant refusal of the application. Additional planting adjacent to the base of the decking, as outlined in the proposed landscaping scheme, would help to reduce the visual impact of the decking and this could be secured, and retained, by way of a condition. In addition, the removal of the weeds that are currently growing between the decking and the front boundary fence and the provision of an evergreen hedge in this area would visually improve the current decking. It is worth noting that there have been no objections from local residents to the proposed development.
2. The Impact Of The Proposal On The Living Conditions Of Neighbouring Residents
- 5.12 The decking is located to the front and side of the dwelling and is in close proximity to the boundary with adjacent residential dwellings. The residential properties on the opposite side of Dowbeck Road, which are at a lower level than the decking, are 15m away from the front of the decking. This distance is adequate to ensure that the living conditions of the occupiers of these properties are not harmed, through loss of privacy.
- 5.13 The boundary with 5 Dowbeck Road lies in close proximity to the decking, which is 1.1m high at this point. There is a 2m high solid wooden fence between the properties and this reduces the impact of the decking. Whilst the height of the decking would enable a degree of overlooking of the neighbouring garden (No. 5), this is not significantly worse than would occur without the decking being present, given the difference in ground levels between the two properties. In terms of direct overlooking of the property itself, this would be limited due to the angle at which the dwelling sits in relation to the decking.
3. Other Issues

SCHEDULE A: Applications with Recommendation

09/0130

- 5.14 If the Development Control Committee decides that the decking, as proposed, is unacceptable, enforcement action would then be required and the steps required to remedy the breach would need to be specified.

Conclusion

- 5.15 In overall terms, the visual impact of the decking as it stands is unduly prominent. This could be moderated, provided that the front corner is removed and the evergreen hedge, outlined in the submitted landscaping scheme, is planted along the base of the decking. The decking would not have an adverse impact on the living conditions of the occupiers of adjacent properties by poor design, unreasonable overlooking or unreasonable loss of daylight.
- 5.16 It is clear that this matter is finally balanced. If Members consider that the harm and visual amenity can be remedied by these revised proposals, permission should be granted.

6. Human Rights Act 1998

- 6.1 Several provisions of the above Act can have implications in relation to the consideration of planning proposals, the most notable being:

Article 6 bestowing the "Right to a Fair Trial" is applicable to both applicants seeking to develop or use land or property and those whose interests may be affected by such proposals;

Article 7 provides that there shall be "No Punishment Without Law" and may be applicable in respect of enforcement proceedings taken by the Authority to regularise any breach of planning control;

Article 8 recognises the "Right To Respect for Private and Family Life";

- 6.2 **Article 1 of Protocol 1** relates to the "Protection of Property" and bestows the right for the peaceful enjoyment of possessions. This right, however, does not impair the right to enforce the law if this is necessary;

- 6.3 The proposal has been considered against the above but in this instance it is not considered that there is any conflict. If it was to be alleged that there was conflict it is considered not to be significant enough to warrant the refusal of permission.

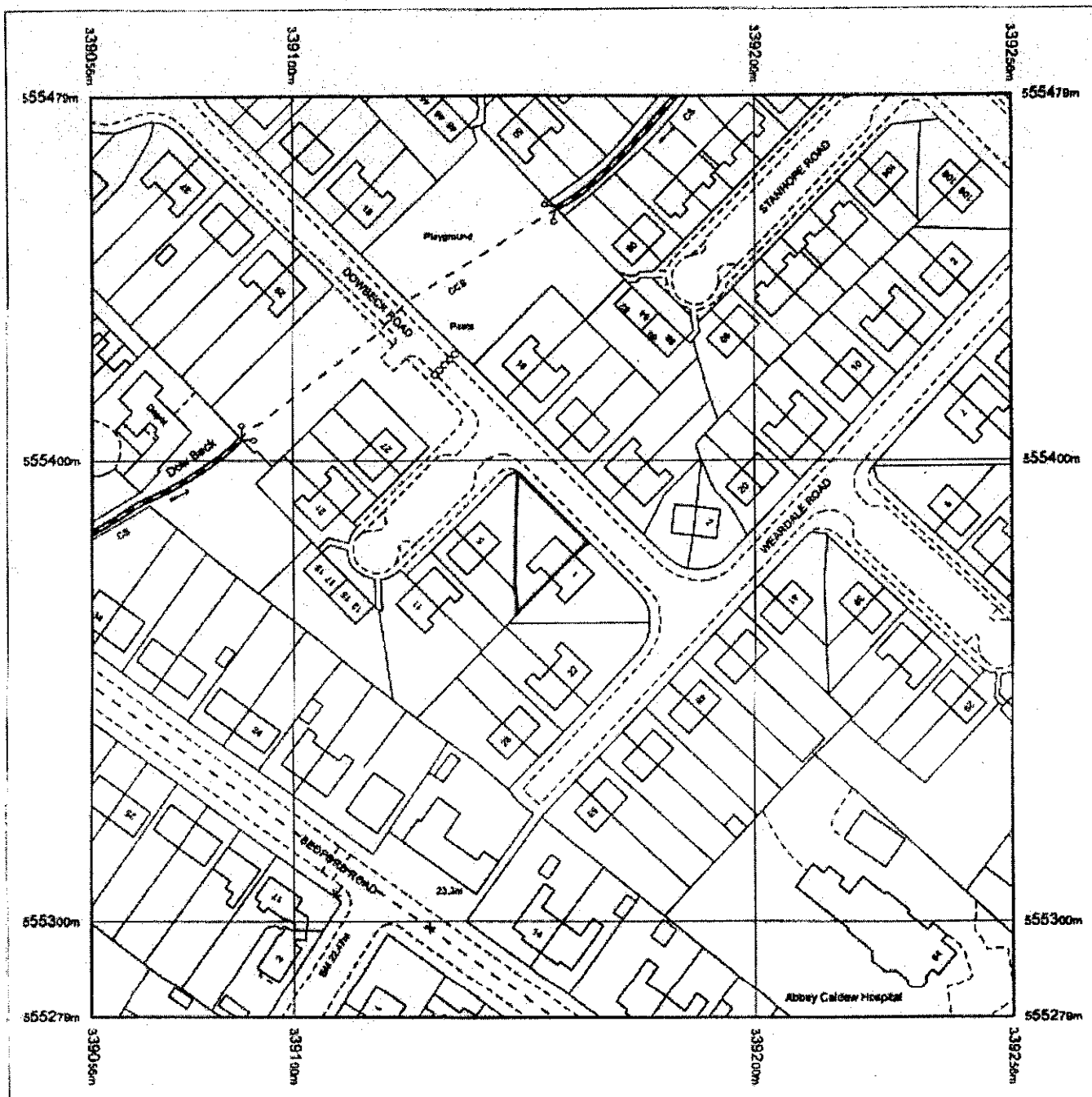
7. Recommendation - Grant Permission

SCHEDULE A: Applications with Recommendation

09/0130

1. All planting comprised in the approved landscaping scheme shall be carried out in the first planting season following the date of this permission and maintained thereafter to the satisfaction of the Council; and any trees or plants which within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the Local Planning Authority gives written consent to any variation.

Reason: To ensure the decking is satisfactory landscaped, in accordance with Policy CP5 of the Carlisle District Local Plan 2001-2016.



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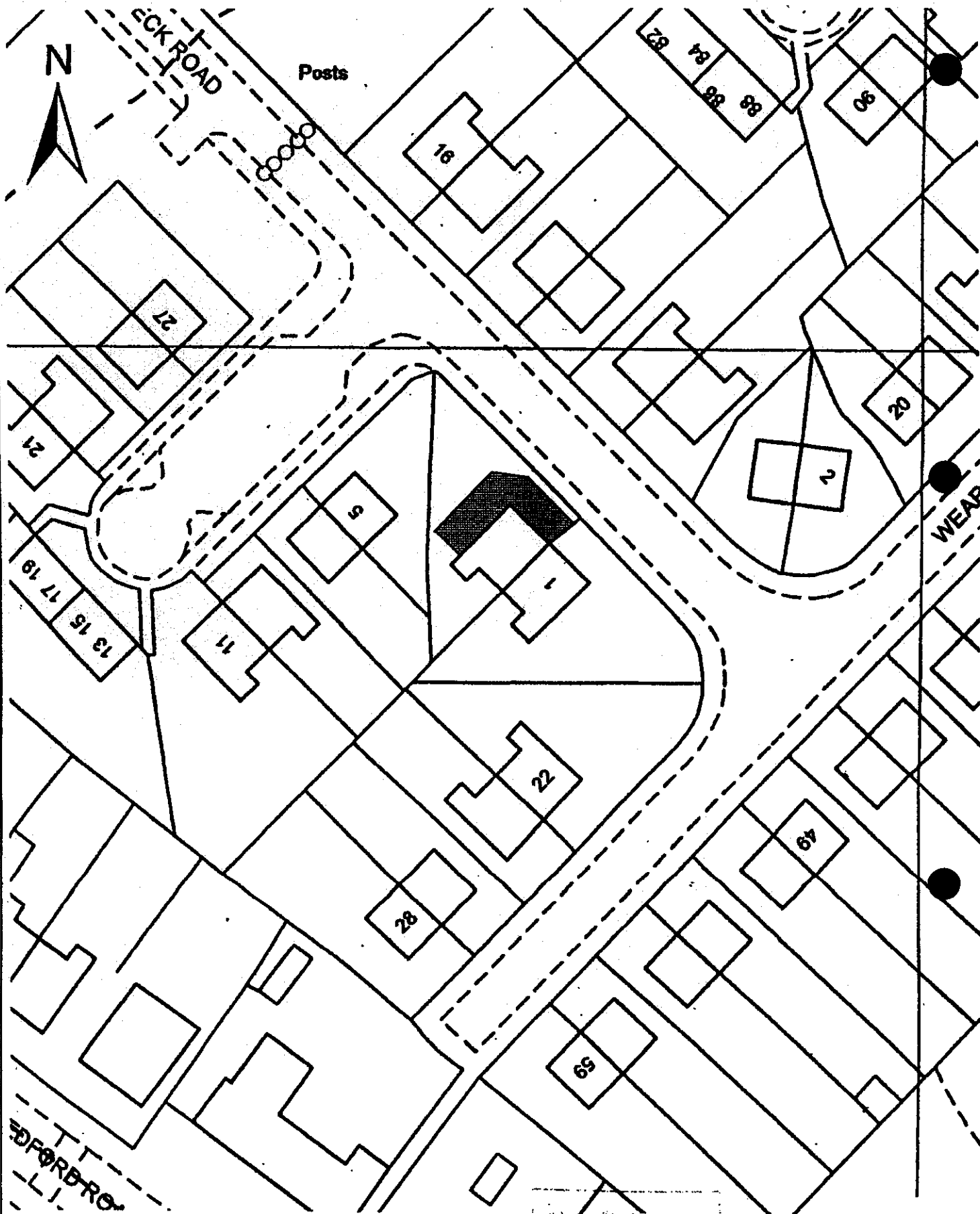
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PROPOSED DECKING

LOCATION PLAN

3 DOWBECK ROAD - CARLISLE

SCALE : 1-1250



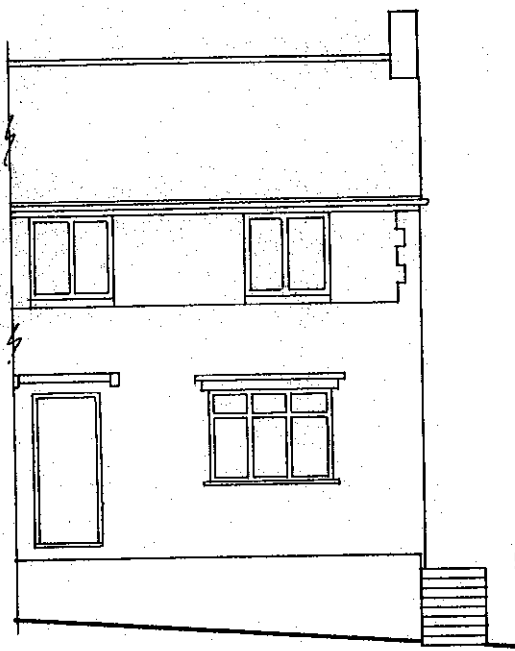
PROPOSED DECKING

23 FEB 2009
09/0130

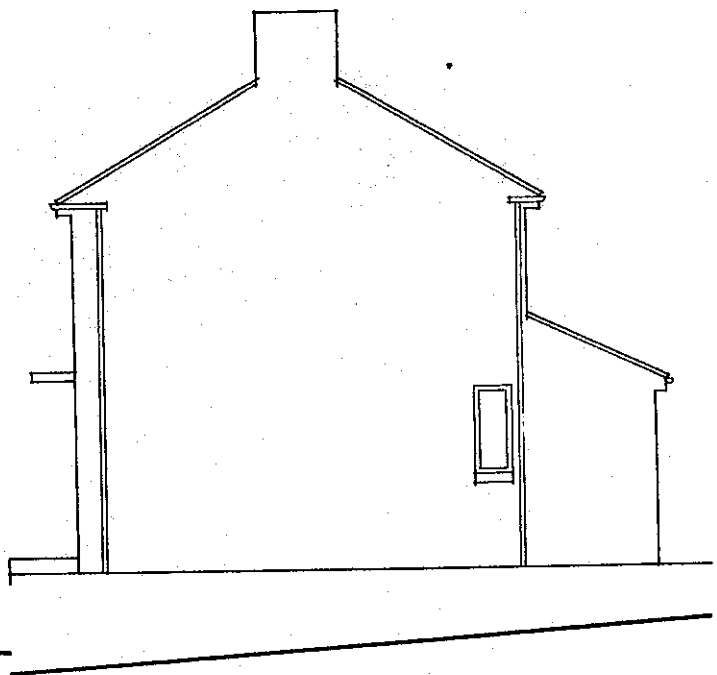
SITE PLAN

3 DOWBECK ROAD - CARLISLE

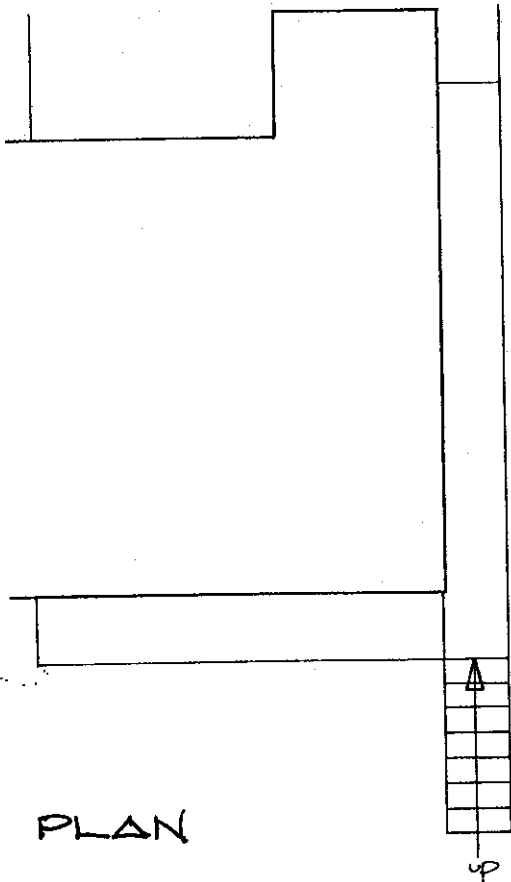
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FRONT ELEVATION

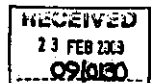


SIDE ELEVATION



PLAN

AS EXISTING

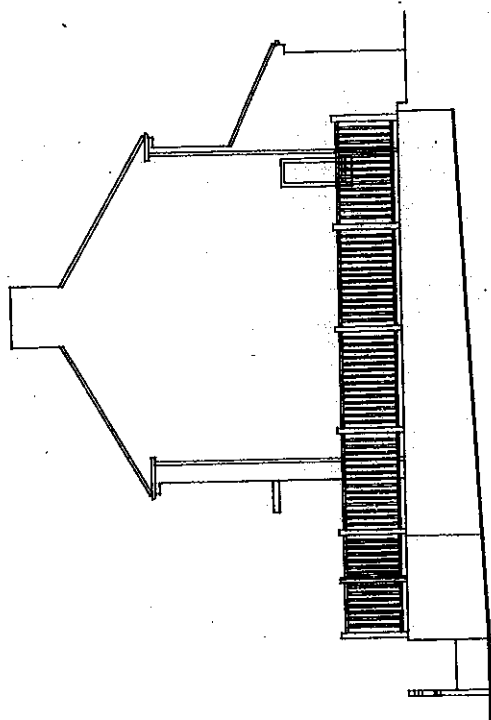


PROPOSED DECKING
3 DOWBECK ROAD - CARLISLE

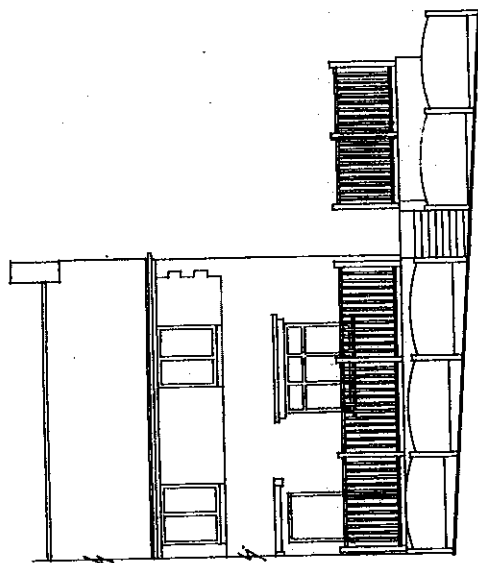
DRG. No. 1767/1

SCALE: 1-50

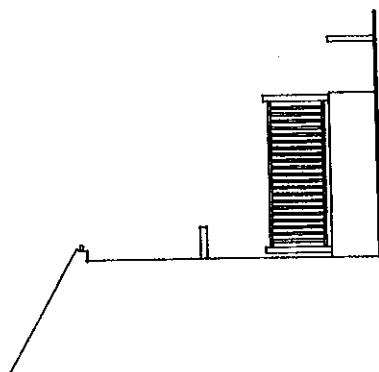
DATE: NOV. 2007



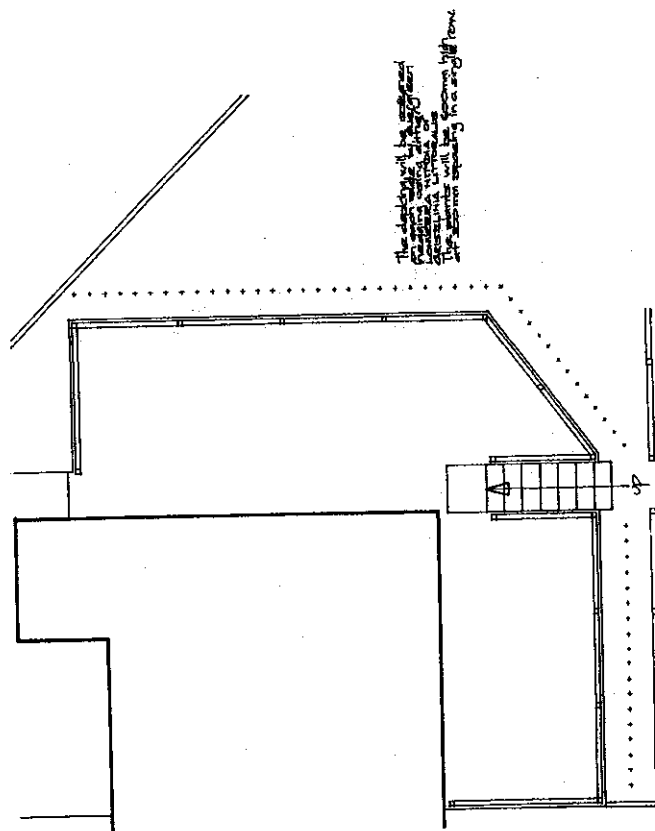
SIDE ELEVATION



FRONT ELEVATION



SIDE ELEVATION



PLAN

RECEIVED
15 MAY 2007
09:03

REVISIONS
20-8-03 Amendment

AS PROPOSED

PROPOSED DECKING
3 DOWBECK ROAD - CARLISLE

Draw No. 1767 / 2B
SCALE: 1-50
DATE: NOV. 2007

Photograph 1: Photograph of No.3 Dowbeck Road taken from the road. Members will note the decking is as refused by committee on 11/7/08 under application reference 08/0337. The decking to the right hand side of the picture is the corner that the applicant has proposed to remove.



Photograph 2: Photograph of No.3 Dowbeck Road taken from the side garden. Members will note the decking is as refused by committee on 11/7/08 under application reference 08/0337. This photograph also shows the corner of decking that the applicant has proposed to remove (to the left of the photo).



Photograph 3: Photograph of No 3 Dowbeck Road taken from outside the adjoining semi detached dwelling looking down Dowbeck Road. Members will note the decking is as refused by committee on 11/7/08 under application reference 08/0337.



SCHEDULE A: Applications with Recommendation

09/0161

Item No: 15

Date of Committee: 29/05/2009

Appn Ref No:
09/0161

Applicant:
Environment Agency

Parish:
Carlisle

Date of Receipt:
05/03/2009 16:03:54

Agent:
AXIS P.E.D. Ltd

Ward:
Castle

Location:
Stephenson Industrial Estate, Willowholme, Carlisle

Grid Reference:
338507 556575

Proposal: Revision To Previous Planning Consent For A Flood Defence Scheme To Include: Revisions Along The Left Bank Of The Eden Including Minor Raising Of Footpath Levels, Reduced Embankment Proposals Which Would Now Include Raising Of Ground Levels Within Land Owned By Wallace Oils; Along Parham Beck Including Revised Wall Location, Inclusion Of A Drawdown Structure And A New Low Flood Embankment; Provision Of A Flood Gate At The Sands Centre; The Installation Of A 4 Metre High CCTV Off Viaduct Estate Road; Alterations Regarding 23-40 The Maltings; Revisions At Little Caldew Pumping Station (BT Yard); The Showman's Guild, Willowholme; And Right Bank Of River Eden At Etterby.

Amendment:

1. Additional plans received on 3rd April 2009 concerning the provision of a flood gate at the "rear" of the Sands Centre.
2. Additional details received concerning right bank of River Eden at Etterby/Stainton; the Showman's Guild Quarters at Willowholme Industrial Estate; the Little Caldew Pumping Station; 23-40 The Maltings; and off Viaduct Estate Road.

REPORT

Case Officer: Angus Hutchinson

Reason for Determination by Committee:

1. Constraints and Planning Policies

Site Of Special Scientific Interest

The proposal relates to land or premises situated within or adjacent to a Site of Special Scientific Interest.

SCHEDULE A: Applications with Recommendation

09/0161

Site Of Nature Conservation Significance Public Footpath

The proposal relates to development which affects a public footpath.

Health & Safety Executive Consultation

The proposal relates to development involving or affected by hazardous substances or noise.

Local Plan Pol DP2 - Regeneration

Local Plan Pol DP6 - Carlisle Northern Developmnt Route

Local Plan Pol CP1 - Landscape Character

Local Plan Pol CP2 - Biodiversity

Local Plan Pol CP3 - Trees and Hedges on Development Sites

Local Plan Pol CP4 - Agricultural Land

Local Plan Pol CP5 - Design

Local Plan Pol CP6 - Residential Amenity

Local Plan Pol CP11-Prot.Groundwaters &Surface Waters

Local Plan Pol CP13 - Pollution

Local Plan CP15 - Access, Mobility and Inclusion

Local Plan Pol CP16 -Public Trans.Pedestrians & Cyclists

Local Plan Pol CP17 - Planning Out Crime

Local Plan Pol LE2 - Sites of Special Scientific Interest

Local Plan Pol LE3 - Other Nature Conservation Sites

Local Plan Pol LE4 - River Corridors

Local Plan Pol LE27- Developed Land in Floodplains

Local Plan Pol LE30 - Derelict Land

Local Plan Pol LC3 - Amenity Open Space

Local Plan Pol LC8 - Rights of Way

2. Summary of Consultation Responses

Hadrian's Wall Heritage Ltd: in relation to originally submitted plans had several issues of concern regarding the effect that the works may have on Hadrian's Wall Path National Trail with regard to the following points:

- the design of the path that is to be raised, in particular its width;
- the access points to the path for when maintenance is necessary;
- the path surface material and its future maintenance, also liability for maintenance during its early years until it is established;
- landscaping in the Willowholme area; this is highly desirable bearing in mind the eyesore that the area presents to the thousands of overseas visitors in particular who use the National Trail;
- the future of the historic bone mill that is near to the Waverley viaduct;
- how the National Trail ties into the Sustrans route; you will not be aware of agreements made between the former Countryside Agency (now Natural England) in 2003 about both the short and long-term plans for the Route 72 cycleway.
- the diversion itself; this must be very clearly signed, with information for walkers.

Cumbria County Council (Rights of Way Officer): the public right of way adjacent to the River Eden currently enjoys two separate paths that are used by pedestrians and cyclists. The proposal states a 2 metre path will be provided; with the dual usage it is considered that a 2.5 metre wide path is provided to cater for this usage.

The path adjacent to the Eden also carries the Hadrian's Wall National Trail, this trail attracts some 7000 users through the summer months, in the proposals there is no mention of a signed diversion for footpath users. Could details of such a diversion and proposed signage please be supplied to the County Council's Countryside Section and Hadrian's Wall Heritage Ltd prior to any closure.

In the case of the second footpath away from the Eden it is proposed that steps are installed at the Eden end of the proposed works; with regard to the DDA Act 2005, would it be possible to ramp this access with a gradient no greater than 1:12?;

County Archaeologist: it is noted that the proposals affect an area that is legally protected as a Scheduled Ancient Monument and thus recommend that English Heritage is consulted.

Notwithstanding any comments from English Heritage, it is confirmed that the archaeological conditions placed on earlier planning consents relating to the elements of the scheme outside the Scheduled Monument are still considered appropriate;

County Council (Highway Authority): comments awaited;

English Heritage: will provide substantive advice on this application by 12th June -

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the extra time is necessary due to staff absence;

Natural England: comments awaited;

Ramblers Association: comments awaited;

Open Spaces Society: comments awaited;

Environmental Services (Green Spaces): comments awaited;

Environmental Services (Environmental Quality): comments awaited;

United Utilities: comments awaited;

Development Services (Property): comments awaited;

Kingmoor Parish Council: comments awaited.

3. Summary of Representations

Representations Received

Initial:

Consulted:

Reply Type:

Ltd, 3 West Street	19/03/09
15 Caldew Maltings	08/05/09
16 Caldew Maltings	08/05/09
17 Caldew Maltings	08/05/09
18 Caldew Maltings	08/05/09
19 Caldew Maltings	08/05/09
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23 Caldew Maltings	08/05/09
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41 Caldew Maltings	08/05/09
42 Caldew Maltings	08/05/09
43 Caldew Maltings	08/05/09

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	44 Caldew Maltings	08/05/09
	45 Caldew Maltings	08/05/09
	46 Caldew Maltings	08/05/09
	47 Caldew Maltings	08/05/09
	48 Caldew Maltings	08/05/09
	49 Caldew Maltings	08/05/09
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	64 Caldew Maltings	08/05/09
	65 Caldew Maltings	08/05/09
	66 Caldew Maltings	08/05/09
	Property Manager	13/05/09
	Showman's Quarters	13/05/09
	Showman's	13/05/09
Quarters	Eden View Farm	13/05/09
	Stainton House Farm	13/05/09
	PO Box 47859	13/05/09
	81 Newgate	13/05/09
Street	Northern House	13/05/09
	c/o Able UK Ltd	19/03/09
	Stephenson Industrial Estate	19/03/09
	The Sands Centre	06/04/09
	Eden View	06/04/09
	Stainton Farm	08/05/09
	1 Stainton Road	08/05/09
	1 Caldew Maltings	08/05/09
	2 Caldew Maltings	08/05/09
	3 Caldew Maltings	08/05/09
	4 Caldew Maltings	08/05/09
	5 Caldew Maltings	08/05/09
	6 Caldew Maltings	08/05/09
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	11 Caldew Maltings	08/05/09
	12 Caldew Maltings	08/05/09
	13 Caldew Maltings	08/05/09
	14 Caldew Maltings	08/05/09

- 3.1 This application has been advertised by means of press and site notices and the direct notification of the owners/occupiers of neighbouring properties. At the time of preparing the report no objections have been received.

4. Planning History

- 4.1 In 2007, under planning reference 06/1473, Full Planning Permission was granted for flood defence improvements for the River Caldwes and Eden (the Caldew and Carlisle City flood defence scheme).
- 4.2 In 2008, under planning reference 07/1389, Full Planning Permission was granted for construction of flood alleviation scheme along the right bank of the river caldew, adjacent to Bousteads Grassing, comprising embankment and walls (amendments to scheme approved under ref 06/1473).
- 4.3 In 2008, under planning reference 08/0112, full planning permission was granted for construction of FAS at various locations along the Rivers Caldew and Eden. The permission amended, in part, the previously consented Caldew And Carlisle City Flood Alleviation Scheme (ref: 06/1473) by: enhancement of Fairy Beck; revision of flood gate, flood defence wall & telemetry control box at Holme Head Weir; realignment & regrading of cycle track on right bank of Denton Street Bridge; widening of embankment, revised steps/ramp, access ramp & site compound from Denton Street Bridge To South Vale Bridge; telemetry kiosk & realignment of defence wall at Metcalfe Street; temporary site compound off Graham Street; revised access, telemetry kiosk & realignment of defence wall to rear of Dunelm; revised alignment of embankment to Trinity School; replacement of existing Swifts Driving Range; revised location of defence wall & car park entrance & new telemetry kiosk from Turf Tavern to Swifts Bank; revised wall alignment, play areas & extension of defence wall at The Sands Centre; revised location of flood defence, access ramps/steps from Hardwicke Circus subway to Bitts Park; revised location & form of flood defences from Bitts Park to Dacre Road; ground raising to Dacre Road & adjacent paths; reconfiguration of Dacre Road Car Park including extension with revised access.
- 4.4 Under applications 08/1038, 08/1081, 08/1259, 09/0058 and 09/0072 permission has been given to further revise the proposed flood defences.
- 4.5 Applications seeking to discharge relevant conditions have been made under 08/0398, 08/0460, 08/0508 and 08/1174.

5. Details of Proposal/Officer Appraisal**Introduction**

- 5.1 In December 2005, under planning application 05/1024, planning permission was given for a scheme of flood defence improvement works associated with the east of the City along sections of the Rivers Eden and Petteril. In May 2007, application 07/0345, planning permission was given for revisions to the scheme approved under 05/1024. The works approved under

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05/1024 and 07/0345 representing Phase 1 of a two-phase project comprising the Carlisle Flood Alleviation Scheme (FAS).

- 5.2 In March 2007, application reference numbers 06/1473 and 07/0090, planning permission and listed building consent were given for proposed flood defence improvements associated with the west of the City along the River Caldew and the River Eden downstream from Eden Bridge, i.e. Phase 2 of the FAS.
- 5.3 In addition, under application reference numbers 07/0706 and 08/0029 planning permission has been given for two "missing" elements in the aforementioned flood defences on land adjacent to Etterby Terrace/Eden Place and Tilbury Road.
- 5.4 The flood defences needing to be also viewed in the context of United Utilities having commenced work on upgrading the sewer network in Warwick Road/Victoria Place (appn. ref. no. 08/9027/CTY and 08/9034/CTY) and Willowholme.
- 5.5 Under application numbers 07/1389, 08/0112, 08/1038, 08/1081, 08/1259, 09/0058 and 09/0072 permission has subsequently been given for amendments to Phase 2 of the FAS.

Background

- 5.6 This application seeks approval for further amendments to the Phase 2 FAS with regard to eight sections.

1. Left bank of the River Eden adjacent to Stephenson Industrial Estate - reduction in the embankment scale utilising existing buildings as part of the flood defence and ground raising at Wallace Oils up to a maximum of 1 m. This is apparently achievable because of the proposed alterations to the scheme at Stainton also part of this application.

2. Parham Beck - changes in the alignment of the proposed flood wall, the minor realignment of Parham Beck, a new low level flood embankment and drawdown structure inclusive of a debris screen and access platform. The proposed embankment crosses the line of Hadrian's Wall and therefore will require Scheduled Monument Consent.

3. The Sands Centre - provision of a flood gate 2 m wide and 0.85 m in height, supported on brick clad piers with reconstituted stone copings to a height of 1.3 m. The proposed changes are following discussions with Carlisle Canoe Club, the City Council's Access Officer and Carlisle Leisure Ltd to overcome any concerns over access to the River by disabled people.

4. Caldew Riverside, off Viaduct Estate Road - installation of a 4 m high CCTV column and associated camera to enable the Environment Agency to monitor any build up of debris around the piers of the Slew Bridge.

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5. The Maltings - introduction of retractable barriers to picture windows, stepping out of the flood gates from the building facade by 750 mm, and new railing.

6. The Little Caldew Pumping Station - erection of a new control structure to regulate flows within the Little Caldew when the water levels in the Caldew are high. The proposed pumping station has been reduced in size (11 m by 6 m) and moved approximately 25 m upstream which has resulted in the realignment of the flood wall within the gardens of The Maltings.

7. The Showman's Guild Quarters - reconfiguration of layout involving provision of two larger toilet buildings, resurfacing of site, erection of 2.2 m high timber fencing along Willowholme frontage and side elevations, and new tree planting.

8. Right Bank of River Eden At Etterby - removal of a larger section of the existing Waverley Embankment enabling creation of a seasonal wetland area, proposed raised areas for stock refuge, fill in part of floodplain to protect pylon, a 38 m wide grassed "spillway" to allow flood water to drain back into the River.

- 5.7 The application is accompanied by respective Change Assessments and Review of Environmental and Design and Access Statement. The application is also accompanied by an Assessment of Likely Significant Effect on a European Site that concludes the proposed changes are unlikely to have a significant effect on any interest feature of a European site and does not significantly increase the impacts of the overall FAS.

Assessment

- 5.8 When considering this application the relevant policies are DP2, DP6, CP1, CP2, CP3, CP4, CP5, CP6, CP11, CP13, CP15, CP16, CP17, LE2, LE3, LE4, LE27, LE30, LC3 and LC8 of the Carlisle District Local Plan (2001-2016). In addition, the proposal should be viewed in the context of the underlying objectives of the Three Rivers Strategy and the Connect2/Sustrans cycle route.

- 5.9 At the time of considering application 06/1473 it was readily acknowledged that substantial social and economic benefits arise to the City as a whole and to the individual residents and businesses previously affected by the January 2005 flood. On this basis it was considered that the determining issues related to whether the advantages outweighed any disadvantages associated with:

1. the loss of agricultural land;
2. the visual and historic character of areas;
3. the flora and fauna re. SAC and SSSI designation and protected species;

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4. archaeology;
 5. the protection of the setting, architectural and historic importance of any Listed Buildings;
 6. access for people with mobility problems;
 7. provision for cyclists and pedestrians;
 8. the living conditions of neighbouring residents;
 9. the provision of open space and protection of playing fields;
 10. existing rail infrastructure including the freight/rail avoidance line;
 11. the potential future use of the Caldey Riverside area of the City;
 12. an extant planning permission for housing on land at Lime Street; and
 13. security.
- 5.10 Based on the location of the current proposal, the aforementioned issues 9), 10) and 12) are not considered relevant. In regard to the remaining issues it is considered that the proposed revisions should either have a neutral or minor impact in comparison to the already approved scheme although the comments of interested parties are still awaited. At the time of preparing the report, the applicant is also seeking to address the concerns of Hadrian's Wall Heritage Ltd and the County Council's Rights of Way Officer.

Conclusion

- 5.11 In conclusion it is considered that any harm is outweighed by the benefits attributable to the provision of the permanent flood defences. An updated report will be presented to Members during the Meeting. On the basis that no substantive objections are raised, authority to issue an approval will be sought subject to the imposition of the following conditions.

6. Human Rights Act 1998

- 6.1 Several provisions of the above Act can have implications in relation to the consideration of planning proposals, the most notable being:

Article 6 bestowing the "Right to a Fair Trial" is applicable to both applicants seeking to develop or use land or property and those whose interests may be affected by such proposals;

Article 7 provides that there shall be "No Punishment Without Law" and

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may be applicable in respect of enforcement proceedings taken by the Authority to regularise any breach of planning control;

Article 8 recognises the "Right To Respect for Private and Family Life";

6.2 **Article 1 of Protocol 1** relates to the "Protection of Property" and bestows the right for the peaceful enjoyment of possessions. This right, however, does not impair the right to enforce the law if this is necessary;

6.3 The proposal has been considered against the above Protocol of the Act but in this instance, it is not considered that there is any conflict. If any conflict was to be alleged it is not felt to be of sufficient weight to refuse planning permission.

7. **Recommendation** - Grant Permission

2. The development shall be begun not later than the expiration of 3 years beginning with the date of the grant of this permission.

Reason: In accordance with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended by Section 51 of the Planning and Compulsory Purchase Act 2004).

3. The development hereby permitted shall be completed in accordance with the scheme of public art approved under applications 08/0460 and 08/1174 and the outstanding details still required (concerning the proposed treatment of the Bitts Park underpass wall) to be submitted to and approved by the local planning authority in full compliance with condition 3 of 06/1473 and condition 4 of 08/0112.

Reason: In order to ensure that the development makes suitable provision in accordance with Policy LC15 of the Carlisle District Local Plan 2001-2016.

4. The development hereby permitted shall be completed in accordance with the programme of archaeological work approved under application 08/0384 and the outstanding details still required to be submitted to and approved by the local planning authority in full compliance with condition 4 of 06/1473 and condition 5 of 08/0112.

Reason: To afford a reasonable opportunity for an examination to be made to determine the existence of any remains of archaeological interest within the site and for the preservation, examination or recording of such remains; and, ensure that a permanent record is made of the structures of historic interest prior to their alteration as part of the proposed development in accordance with Policies LE5, LE6, LE7, LE8, and LE10 of the Carlisle District Local Plan 2001-2016.

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5. No part of the structure/embankment associated with the flood defence scheme adjacent to the West Coast Main Line (between Hadrians Wall and the property currently occupied by Brown Bros Engineering, Willowholme Industrial Estate) hereby permitted shall be erected/formed until:

a) an archaeological evaluation has been undertaken adjacent to the West Coast Main Line (between Hadrians Wall and the property currently occupied by Brown Bros Engineering, Willowholme Industrial Estate) in accordance with a written scheme of investigation to be submitted to and approved in writing beforehand by the local planning authority;

b) in the event that the results of the evaluation provided for in paragraph a) above reveals that there are remains associated with Hadrians Wall Vallum, the applicant/developer will submit additional details to be approved in writing by the local planning authority allowing for the remains to be preserved in situ; and,

c) archaeological remains, other than those defined in above paragraph b), identified in the evaluation will be recorded in accordance with a written scheme of investigation submitted to and approved by the local planning authority.

Reason: To afford a reasonable opportunity for an examination to be made to determine the existence of any remains of archaeological interest within the site and for the preservation, examination or recording of such remains; and, ensure that a permanent record is made of the structures of historic interest in accordance with Policies LE5, LE6, LE7, LE8, and LE10 of the Carlisle District Local Plan 2001-2016.

6. The development hereby permitted shall be completed in accordance with the scheme detailing the works at Carlisle Castle approved under application 08/0384.

Reason: In accordance with Policy LE6 of the Carlisle District Local Plan (2001-2016).

7. The development hereby permitted shall be carried out in accordance with the replacement bridge details approved under application 08/0508.

Reason: To safeguard the character of the area and ensure the provision of an effective crossing.

8. The development hereby permitted shall be completed in accordance with the floodgates for Phase 1 approved under application 08/0508 and the outstanding details still required to be submitted to and approved by the local planning authority in full compliance with condition 8 of 06/1473 and condition 9 of 08/0112.

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Reason: To safeguard the character of the area in accordance with Policy CP5 of the Carlisle District Local Plan 2001-2016.

9. Any part of the development approved by this permission that is subject to alteration of an agreed method statement or structure that might have an adverse impact on a SAC/SSSI and/or UK Biodiversity Action Plan Habitats or species, must not be commenced until details have been submitted to, and approved in writing by, the local planning authority.

Reason: To ensure that there is no adverse impact on the SAC/SSSI and/or UK Biodiversity Action Plan Habitats or species in accordance with Policy DP7 of the Carlisle District Local Plan 2001-2016.

10. The development hereby permitted shall be completed in accordance with the landscaping details for Phase 1 approved under application 08/0460 and the outstanding details still required to be submitted to and approved by the local planning authority in full compliance with condition 10 of 06/1473 and condition 11 of 08/0112.

Reason: To ensure that satisfactory landscaping/habitat creation scheme(s) is implemented in accordance with Policy CP3 of the Carlisle District Local Plan 2001-2016.

11. The development hereby permitted shall be completed in accordance with the design details of the proposed foot and cycle path construction approved under 08/0508 and the outstanding details still required to be submitted to and approved by the local planning authority in full compliance with condition 12 of 06/1473, condition 4 of 07/1389 and condition 13 of 08/0112.

Reason: To ensure the development supports the objectives of Policies CP16 and LC8 of the Carlisle District Local Plan 2001-2016.

12. The development hereby permitted shall be completed in accordance with the type of stone and brick approved under 06/1473 and 08/1174 and, the coursing pattern of the stone and brick approved under 08/0112.

Reason: To ensure the works harmonise with the existing character of the area and buildings in accordance with Policy CP5 of the Carlisle District Local Plan 2001-2016.

13. The works shall be completed in accordance with the details of the proposed ducting to be provided as originally approved under application 08/0508 and as revised by the development hereby permitted.

Reason: In the interests of public safety.

14. The development hereby permitted shall be completed in accordance with the scheme detailing the control of construction activity at the site(s) and

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construction traffic to and from the site(s) approved under application 08/0384.

Reason: In order to ensure that the construction of the development of this site is undertaken in a manner which minimises its effect on the local environment and the living conditions of neighbouring residents.

15. The development hereby permitted shall be completed in accordance with the details specifying the location of the Denton Street compound as approved under application 08/0384.

Reason: To safeguard the living conditions of neighbouring residents.

16. Before the commencement of development of the embankments at Etterby and Stainton, details shall be submitted to and approved in writing by the local planning authority specifying the extent of the removal and re-modelling.

Reason: For the avoidance of doubt and to safeguard the character of the area.

17. The development hereby permitted shall be completed in accordance with the details specifying the provision of artificial kingfisher nesting boxes along sections of the River Caldw as approved under application 08/0384. The approved details shall be fully undertaken following the completion of that respective phase of the development.

Reason: To ensure that the proposal not only protects but also enhances biodiversity in accordance with PPS 9.

18. The development hereby permitted shall be completed in accordance with the scheme for the provision of bat boxes as approved under application 08/0460. The approved details shall be fully undertaken following the completion of that respective phase of the development.

Reason: To ensure no adverse impact on a favourable status of a European protected species in accordance with the requirements of the Habitats Regulations 1994.

19. The development hereby permitted shall be completed in accordance with the design details and finish of the proposed metal steps as approved under application 08/0508. The approved details shall be fully undertaken following the completion of that respective phase of the development.

Reason: To safeguard the character of the area and ensure the provision of an effective crossing.

20. For the duration of the development works existing trees to be retained shall

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be protected by a suitable barrier erected and maintained in accordance with BS 5837:2005 Trees in relation to Construction Recommendations. Within this protected area there shall be no excavation, tipping or stacking, nor compaction of the ground by any other means.

Reason: To protect trees and hedges during development works.
